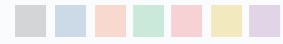




Henry J Lyons

Kilmoney Road, Carrigaline
Strategic Housing Development
Architectural Design Statement
May 2022

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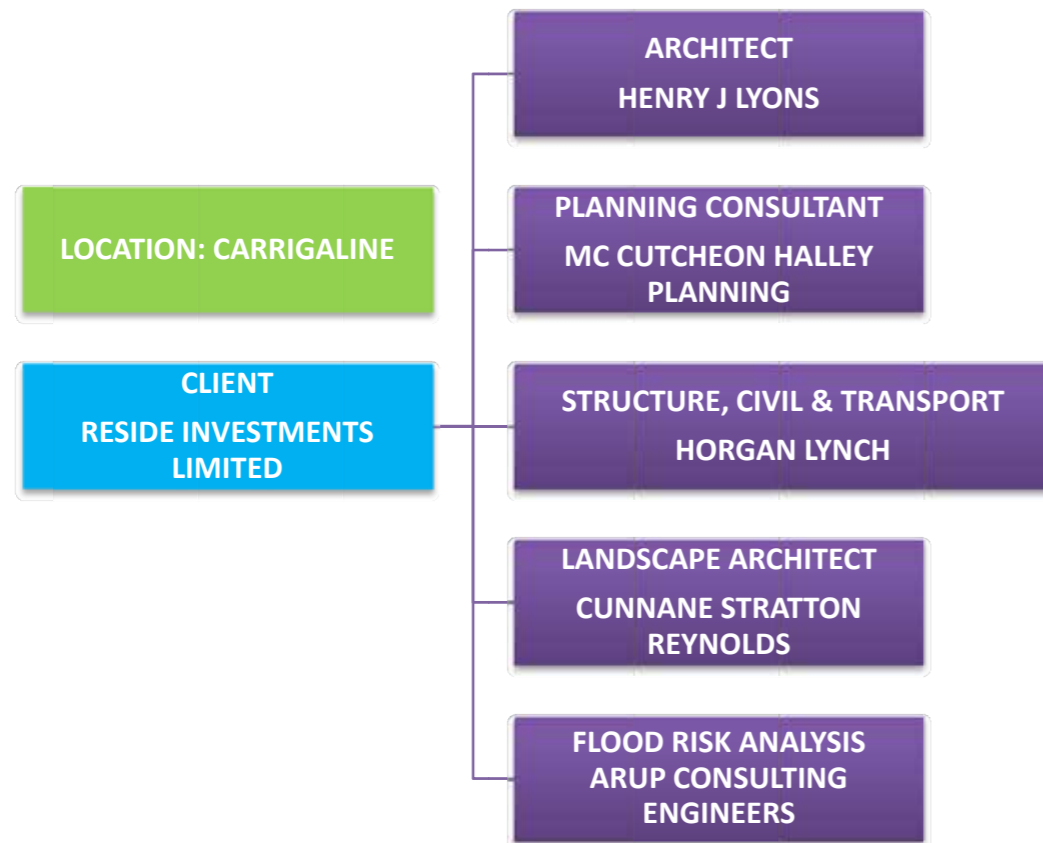
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1.0 DESIGN TEAM



Henry J Lyons



CUNNANE STRATTON REYNOLDS
LAND PLANNING & DESIGN

ARUP

2.0 INTRODUCTION

2.0 INTRODUCTION



DEVELOPMENT OPPORTUNITIES

The proposed development seeks to transform Carrigaline town centre along the East and West axis of the current main street, expanding and centralising development into a more accessible style of town centre that promotes connectivity and public realm.

In keeping with the Cork County Development Plan, the proposal looks to create a more urban style development that is more appropriate to the densities that Carrigaline town has to accommodate, while also fulfilling the requirement of the local community for mixed use development and additional public amenity space.

On behalf of Reside Investments, Henry J Lyons has come together with Mc Cutcheon Haley Planning Consultants, Horizon Engineers, Horgan Lynch Engineers and Cunnane Stratton Reynolds Landscape Architects to create the proposal outlined in this document.

The proposed development presents a number of opportunities:

- The crucial need to deliver residential accommodation in a densely populated town
- Opportunity to deliver a mixed use, town centre development
- The introduction of much needed outdoor, public amenity space to the town centre
- The transformation of the current town centre into one that is more accessible and promotes connectivity

DEVELOPMENT INFORMATION

SITE AREA | 30,701 sq m/ 3 HA

TOTAL NUMBER OF UNITS | 224

DENSITY | 118 UNITS PER HA

1 BED UNITS | 101 NO. | 45%

2 BED UNITS | 111 NO. | 49%

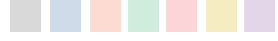
3 BED UNITS | 12 NO. | 6%

CAR SPACES | 255

CYCLE SPACES | 503

3.0 SITE & CONTEXT

3.2 SITE LOCATION - AERIAL IMAGE



Site Extents - Aerial Image

3.3 SITE PHOTOS



1. Location of proposed new relief road.



2. View of Dairygold Co- Op.



3. View of nearby houses.



4. River Owenabue at the North of the site



5. Bank Opposite - Considerate level difference



6. Key Plan of site

3.4 SITE CONTEXT - STREET ELEVATIONS



3.5 SITE TOPOGRAPHY



SITE FEATURES

SITE AREA: 3 Ha

SITE CONDITION: Greenfield Site

SITE GRADIENT: +11.00 (South) to +2.00 (North)

- Previously agricultural land, the site is an open field covered in an expanse of meadow grass. The site slopes from South to North, down to the Owenabue River that curves around the Northern half of the site.
- The site slopes down gently to meet the bank of the river which is in need of some maintenance and natural intervention.
- A large oak tree is located along the southern boundary adjacent to a residential property that will need careful consideration. Other than that, the site is free from any other trees and structures.
- In conjunction with the Western Relief Road, a pumping station has been proposed to be located within the site boundary to the West. Plans to integrate this within the landscape will be of paramount importance to the development.



Topographical Survey of Site

3.6 SITE ROAD STRATEGY

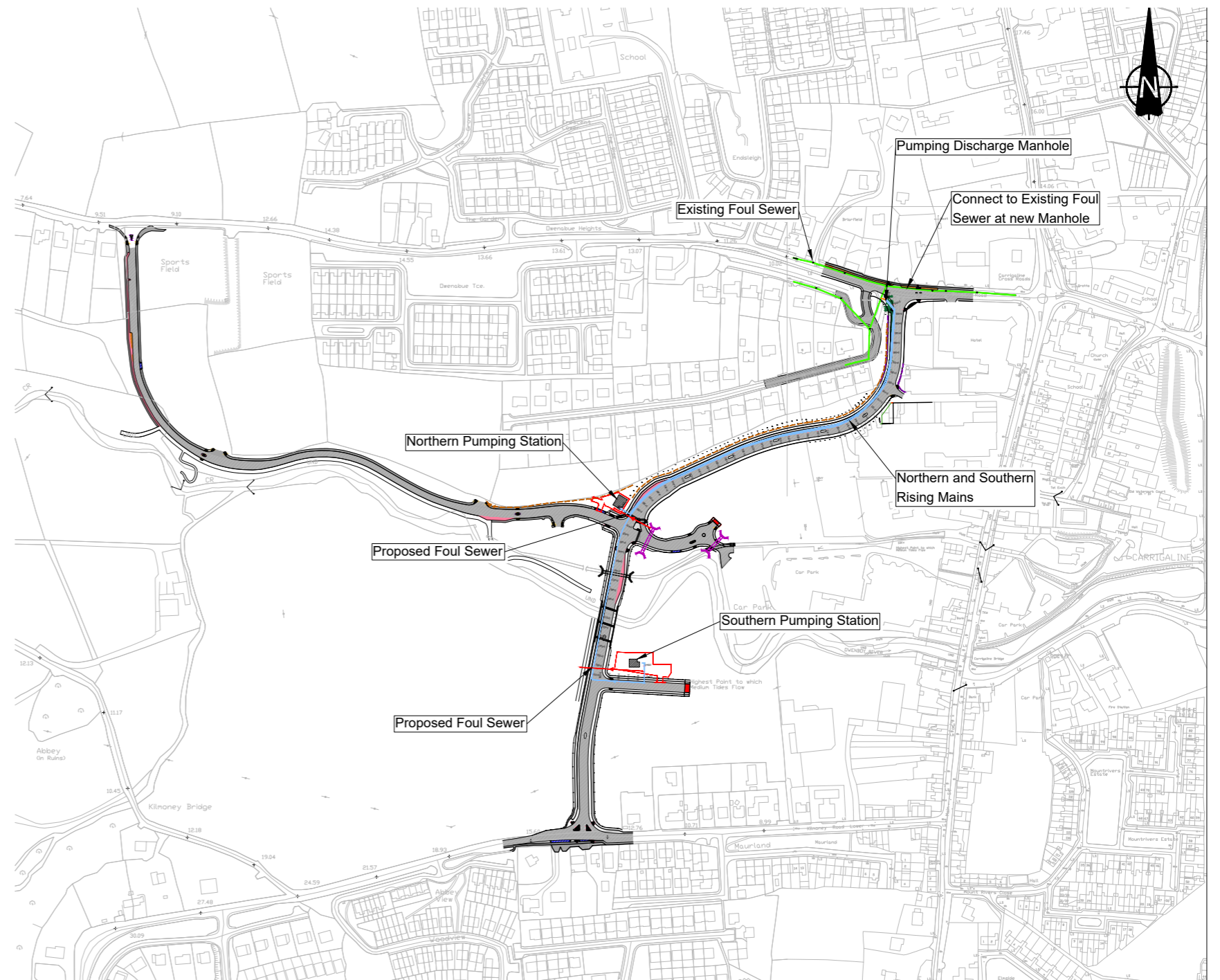


WESTERN RELIEF ROAD

A crucial aspect of this development, is the introduction of the Western Relief Road to Carrigaline. As well as easing congestion within the town, the road will provide accessibility and connectivity to the site as it is located directly along the sites western boundary.

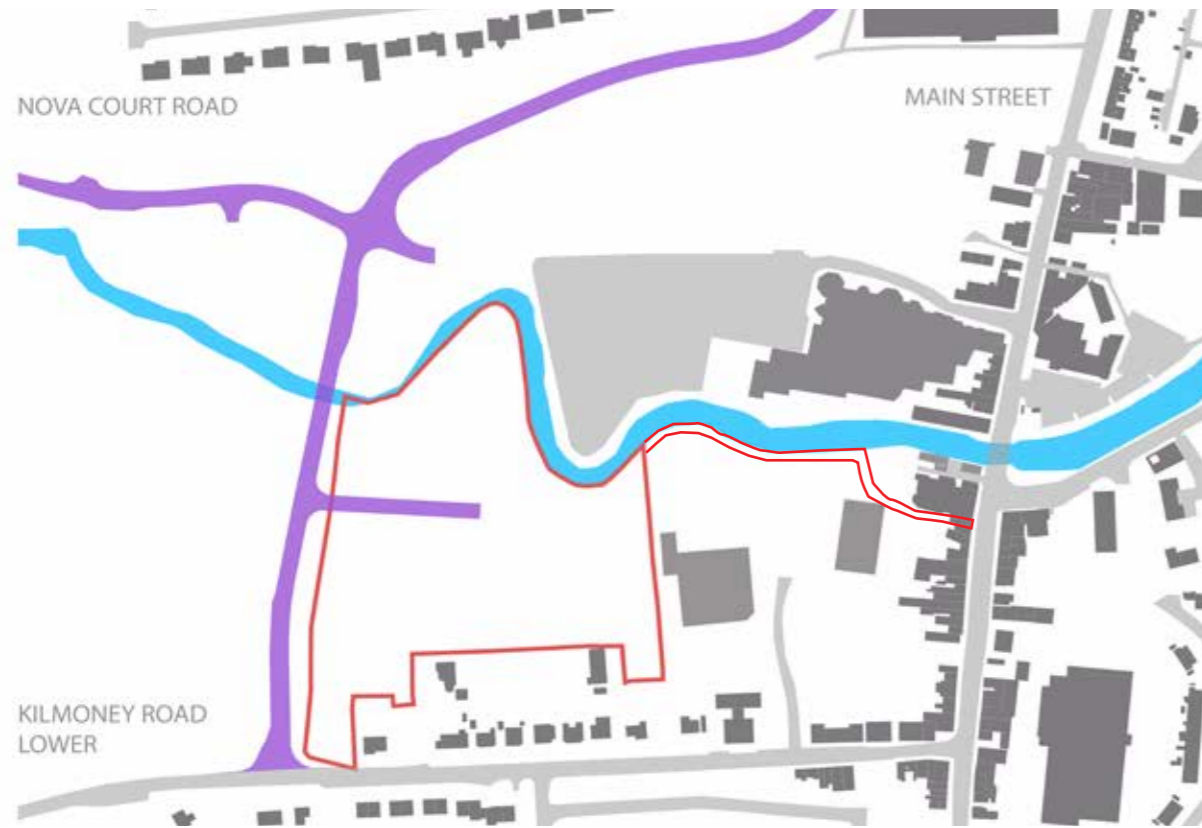
Opportunities linked to the introduction of the Western Relief Road:

- The CMAT notes that there is a potential additional residential yield of 2,380 units, and in order to unlock its development potential, the town needs enhanced public transport connectivity through the Cork Metropolitan Area Transportation Strategy (CMATS), as well as investment in retrofitting infrastructure and services (physical, social and recreational) to improve the quality of life for communities in the town.
- In this context, a new Bus Connects route linking Carrigaline with Ringaskiddy and the City is proposed in the Cork Metropolitan Area Transportation Strategy (CMATS), as is the Lee to Sea Greenway.
- The RSES lists under Part A of the NDP the development of the Carrigaline Western Distribution Road as an enabler to consolidate development within the Carrigaline development boundary and to promote the East to West greenway through the city center.



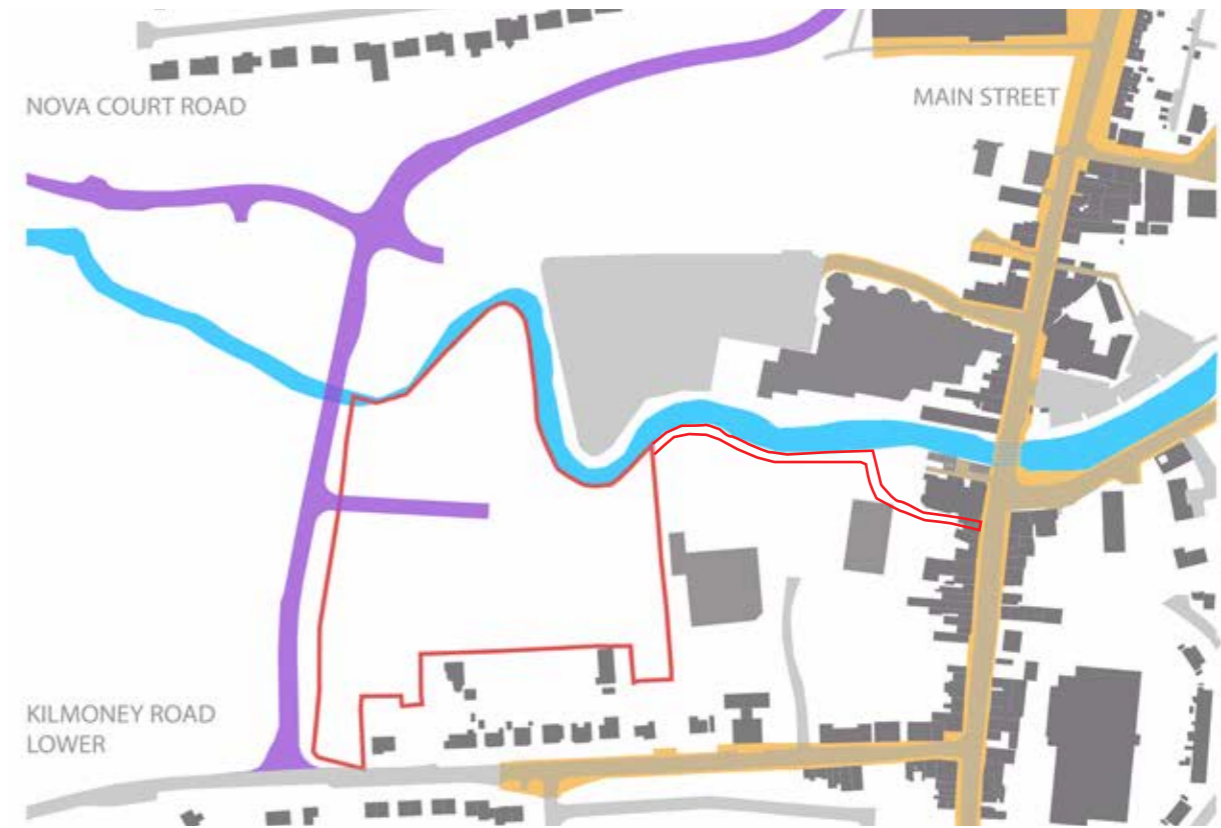
Drawing of Western Relief Road

3.7 SITE CONTEXT ANALYSIS



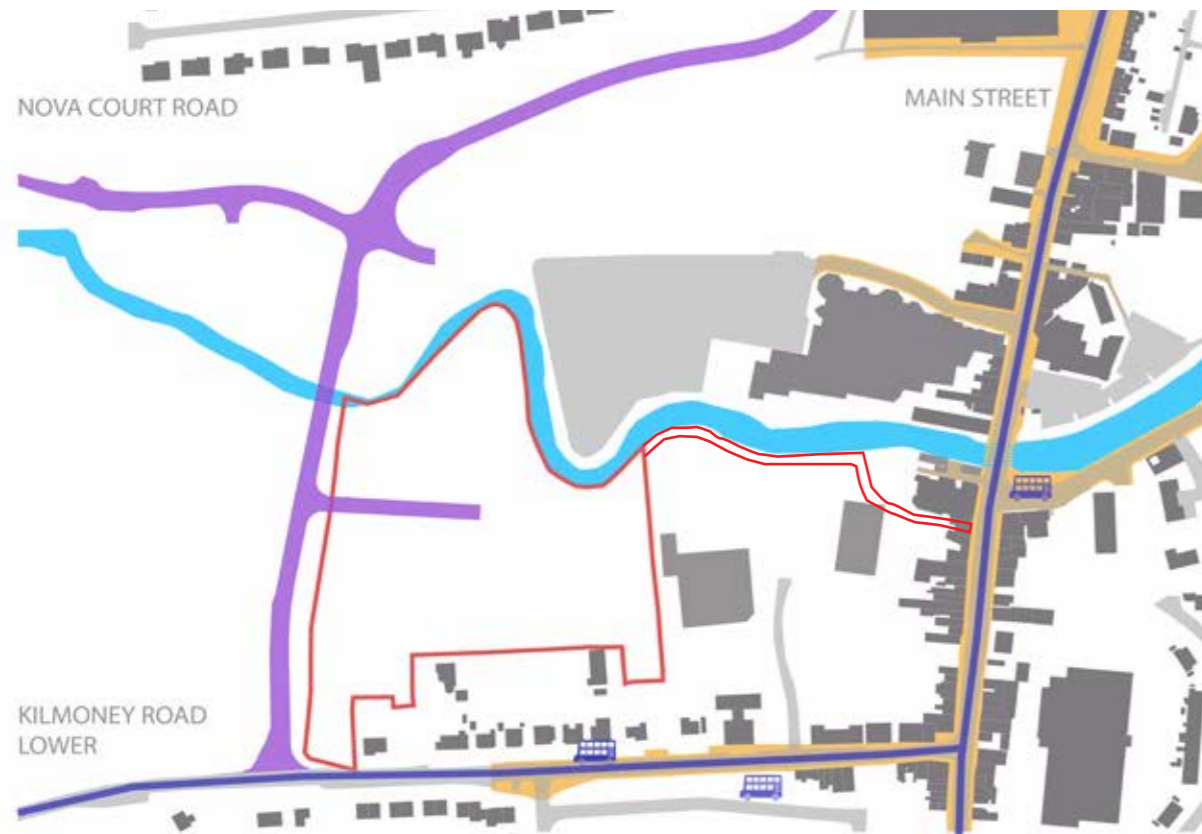
- SITE
- BUILDINGS
- RIVER OWENABUE
- ROAD NETWORK
- PROPOSED RELIEF ROAD

Site Analysis



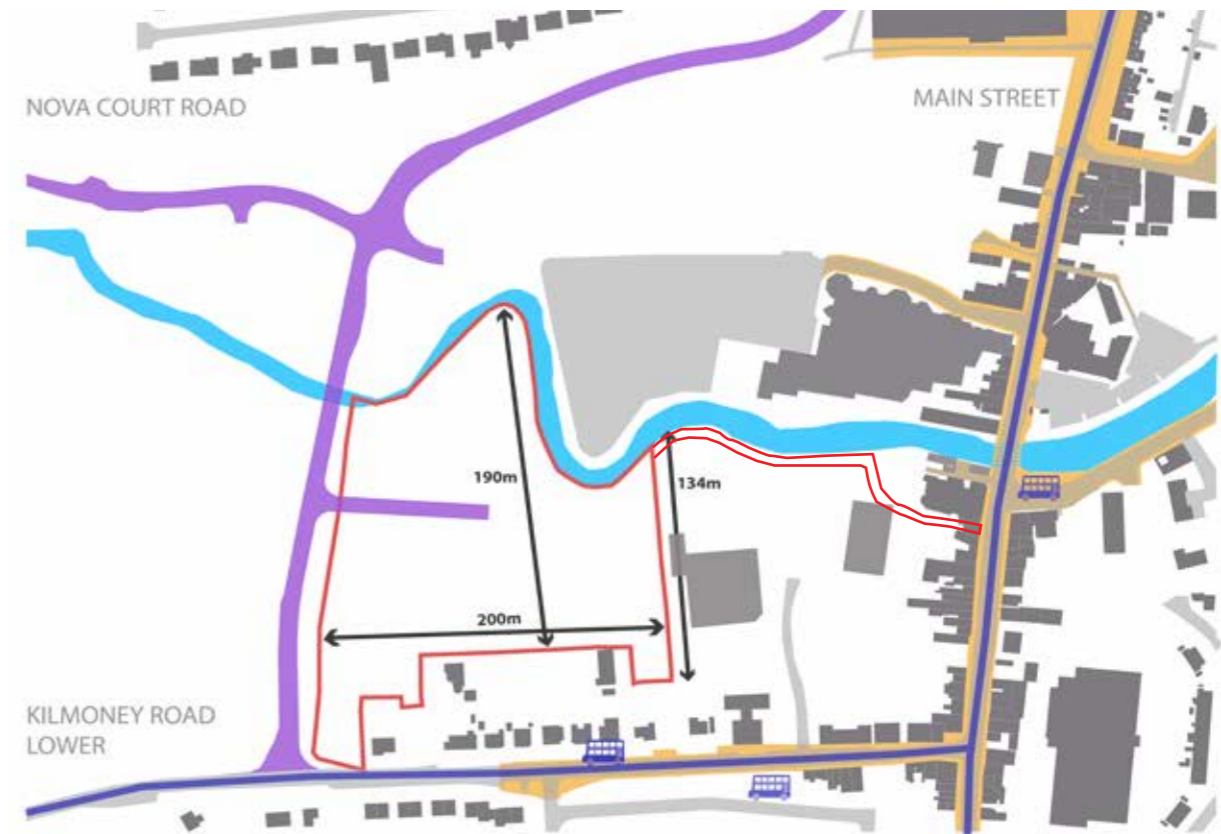
- SITE
- BUILDINGS
- RIVER OWENABUE
- ROAD NETWORK
- PROPOSED RELIEF ROAD
- RETAIL CENTRE

Site demonstrating retail centre



- SITE
- BUILDINGS
- RIVER OWENABUE
- ROAD NETWORK
- PROPOSED RELIEF ROAD
- RETAIL CENTRE
- PUBLIC TRANSPORT ROUTE
- BUS STOPS

Site demonstrating public transport route and stops



- SITE
- BUILDINGS
- RIVER OWENABUE
- ROAD NETWORK
- PROPOSED RELIEF ROAD
- RETAIL CENTRE
- PUBLIC TRANSPORT ROUTE
- BUS STOPS

Site with key dimensions illustrated

4.0 DESIGN STRATEGY

4.1 SITE CONTEXT ANALYSIS STRATEGY

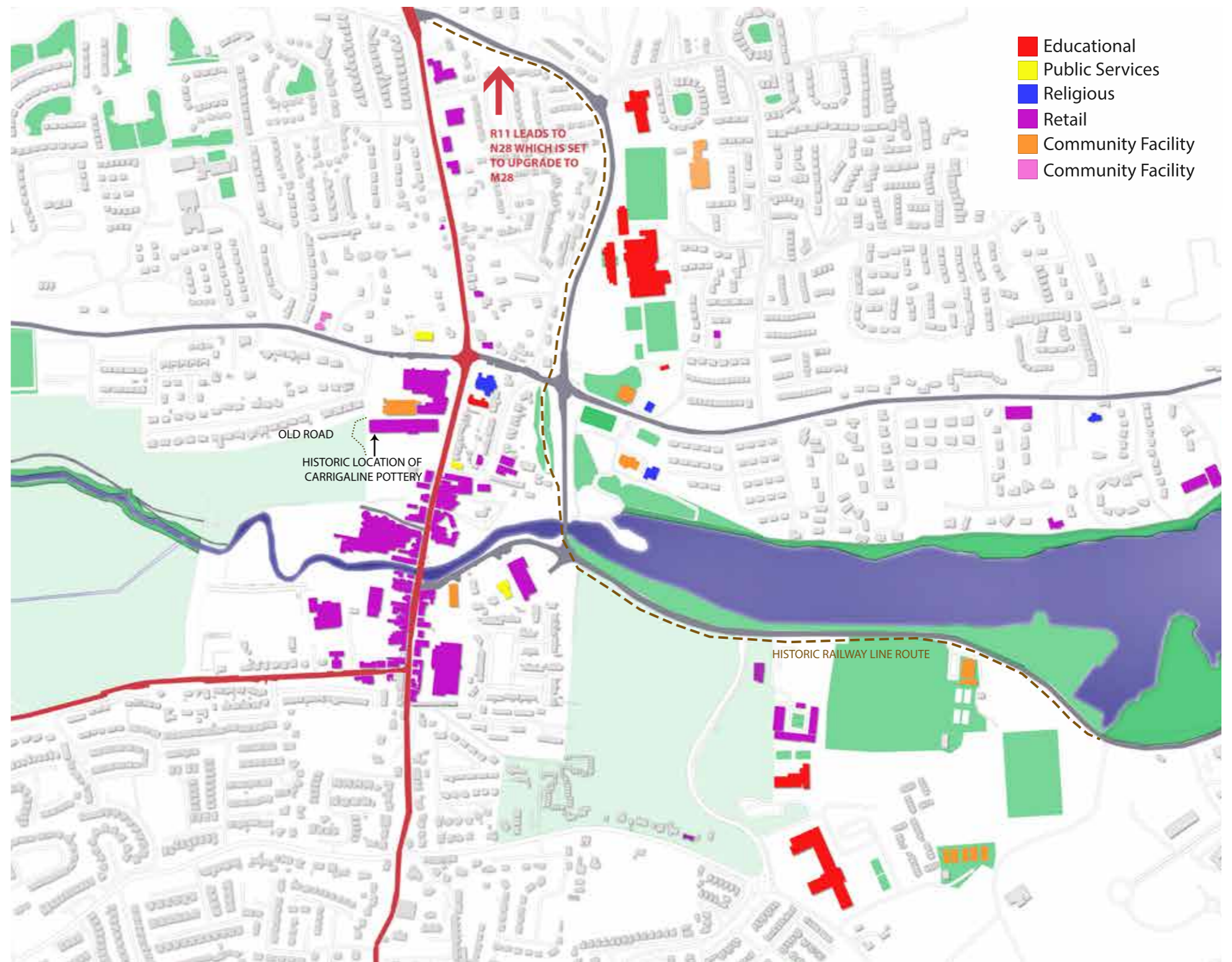


SITE OPPORTUNITIES & CONSTRAINTS

The starting point for the development of the design strategy was a detailed analysis of the site and its wider context in order to identify the key characteristics which create opportunities and cause constraints on site. A greater understanding of the town as a whole, and identifying what it is that the development could provide and add to the town was vital.

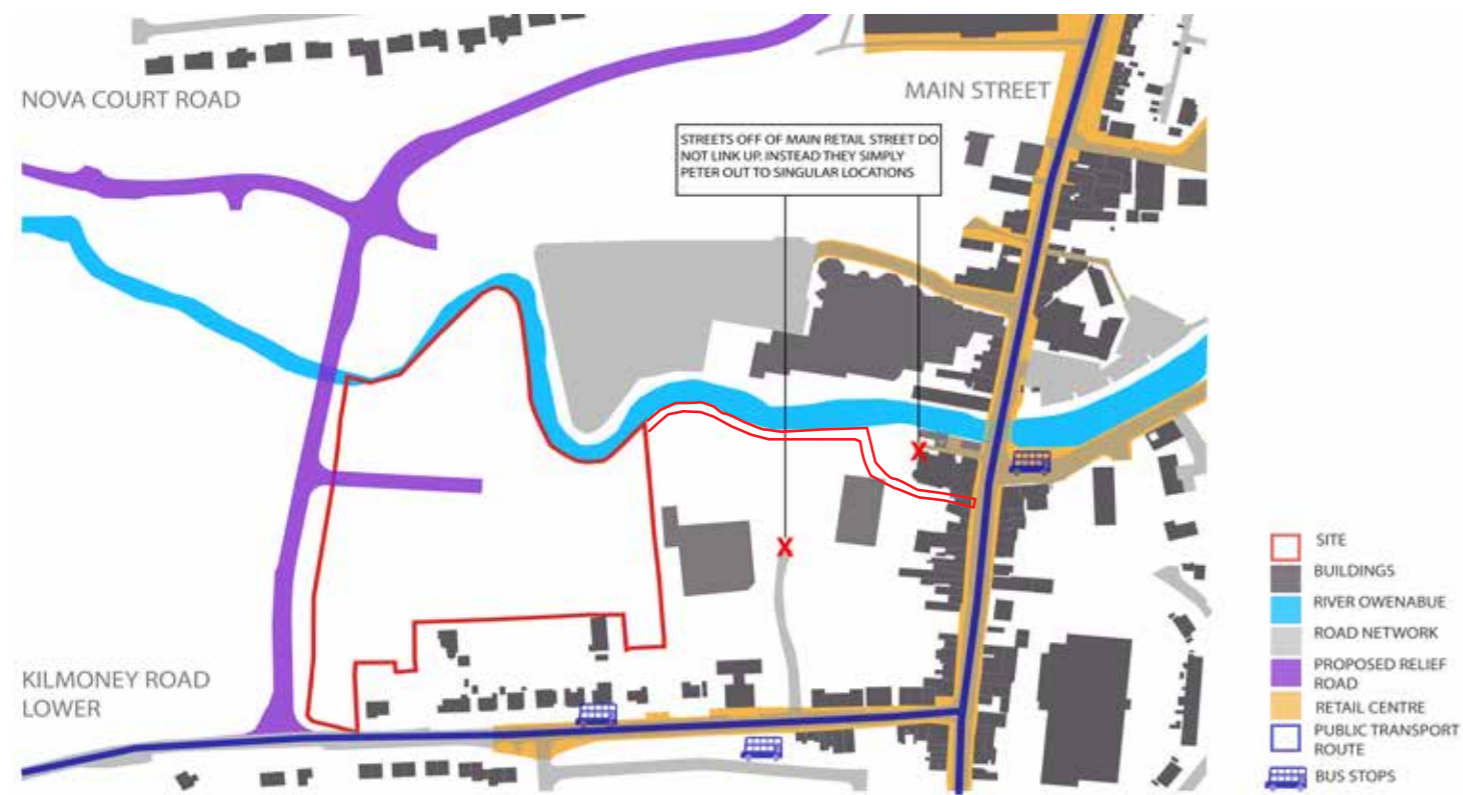
Opportunities that shaped the design strategy:

- The town currently runs from North to South along the main street with little to no permeation either East or West - this creates poor accessibility and connectivity within the town. Expanding the town centre East to West is paramount.
- The town is lacking in public outdoor amenity space on the Western side of the town
- The introduction of the greenway through the town and across the site will improve and enhance connectivity and activate the waterfront
- Public transport routes run close to and around the site
- Large car parks lie adjacent to the site which add to the accessibility of the development
- The proposed scheme to the North of the site will unlock these lands and create connections through to the site and wider town area
- The site location is ideally situated for pedestrian access and commute within the town

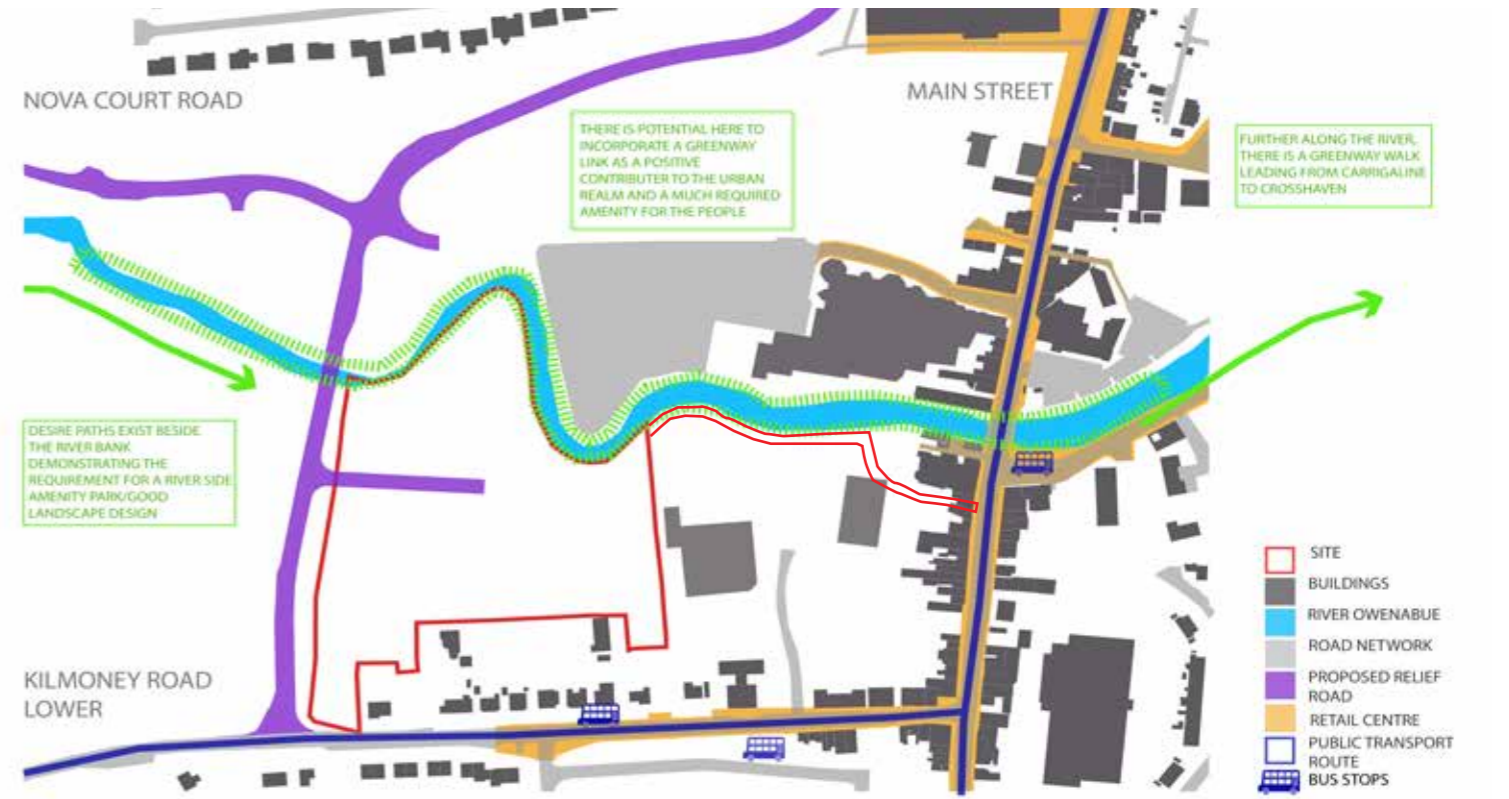


Wider context analysis in relation to site

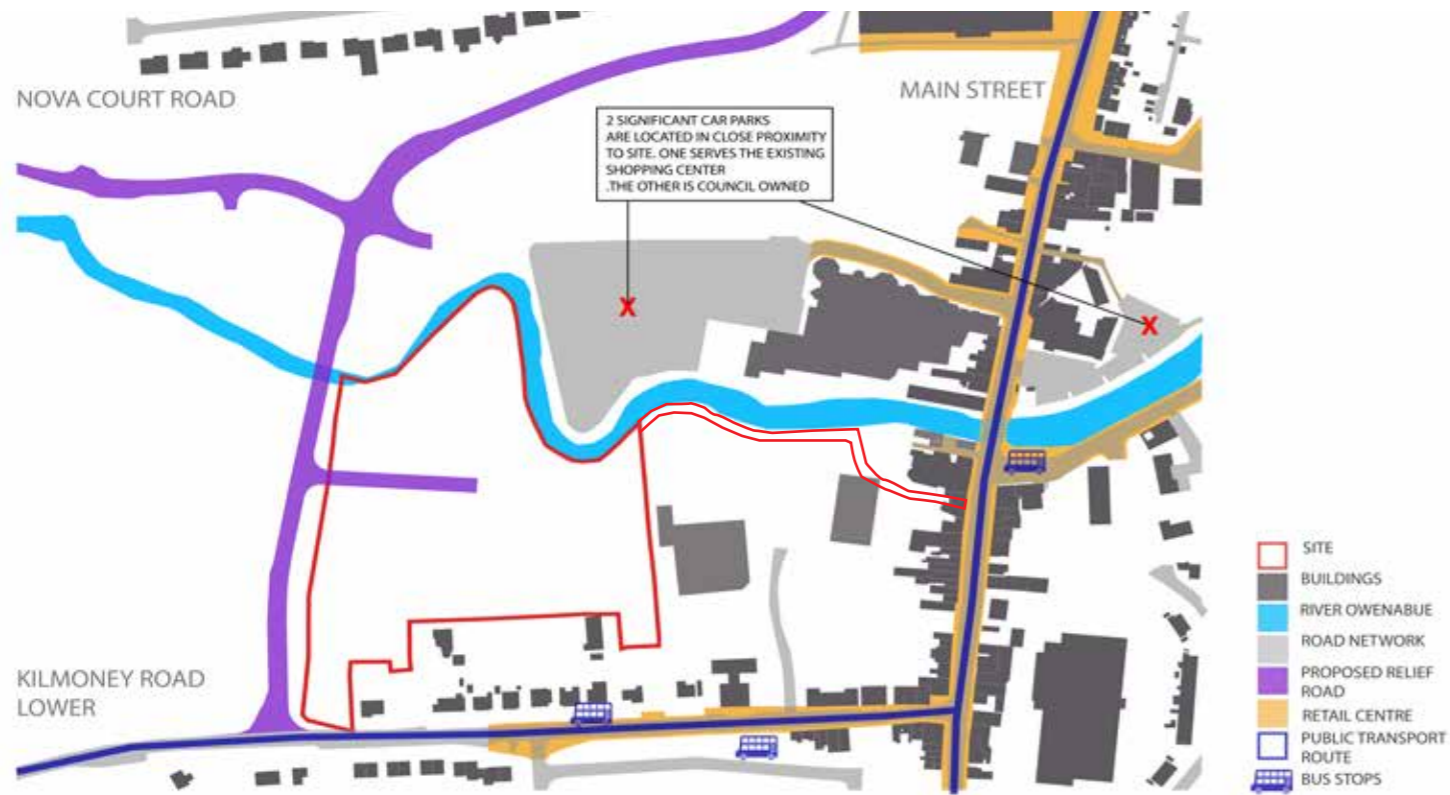
4.2 EMERGING OPPORTUNITIES



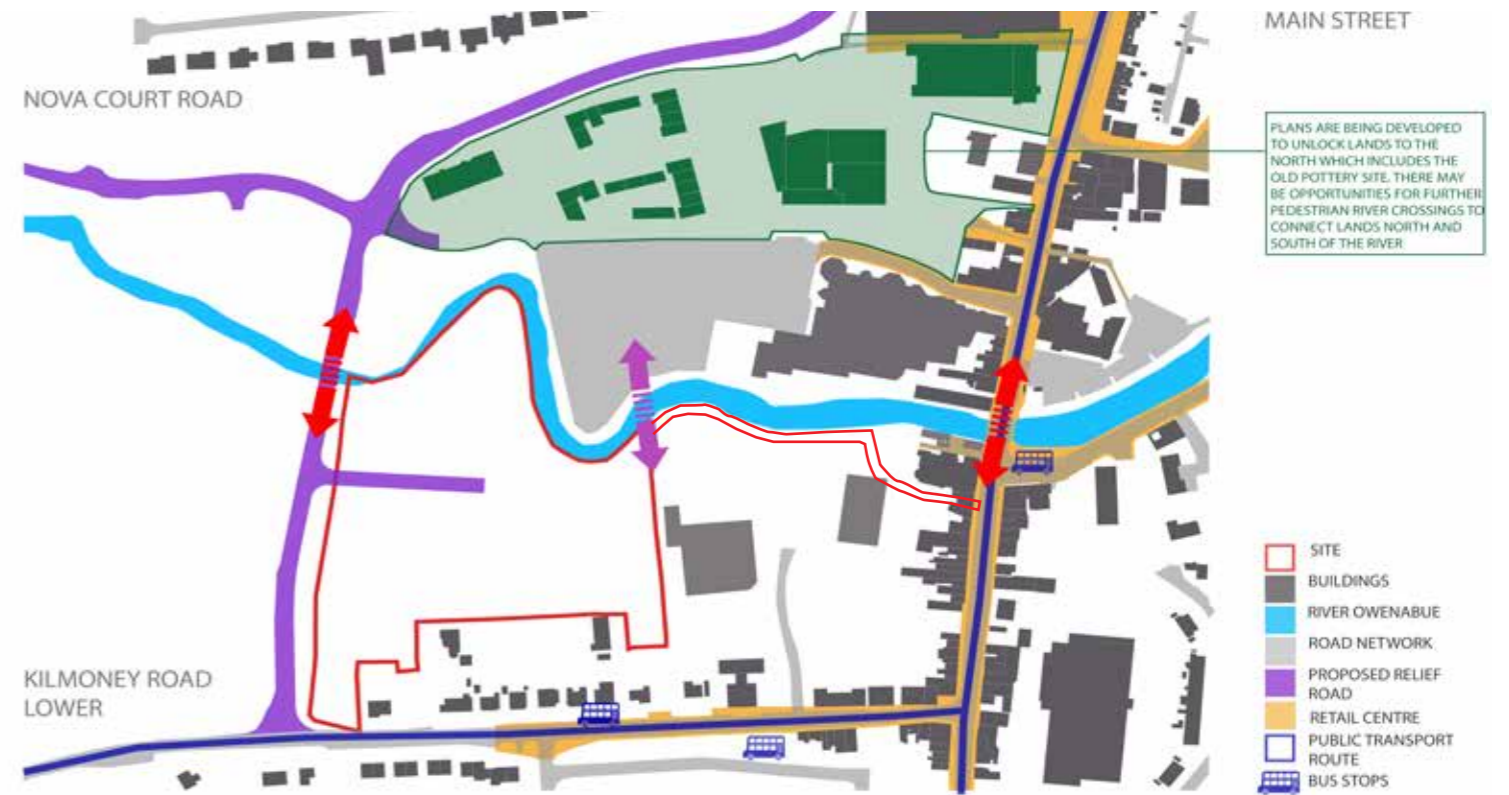
Emerging Opportunities - Street/town disconnect from East to West



Emerging Opportunities - Introduction of greenway through town and across site



Emerging Opportunities - Car parks in close proximity to site provide accessibility



Emerging Opportunities - Development of land to the North will enhance connectivity

4.3 STREET CONNECTIVITY

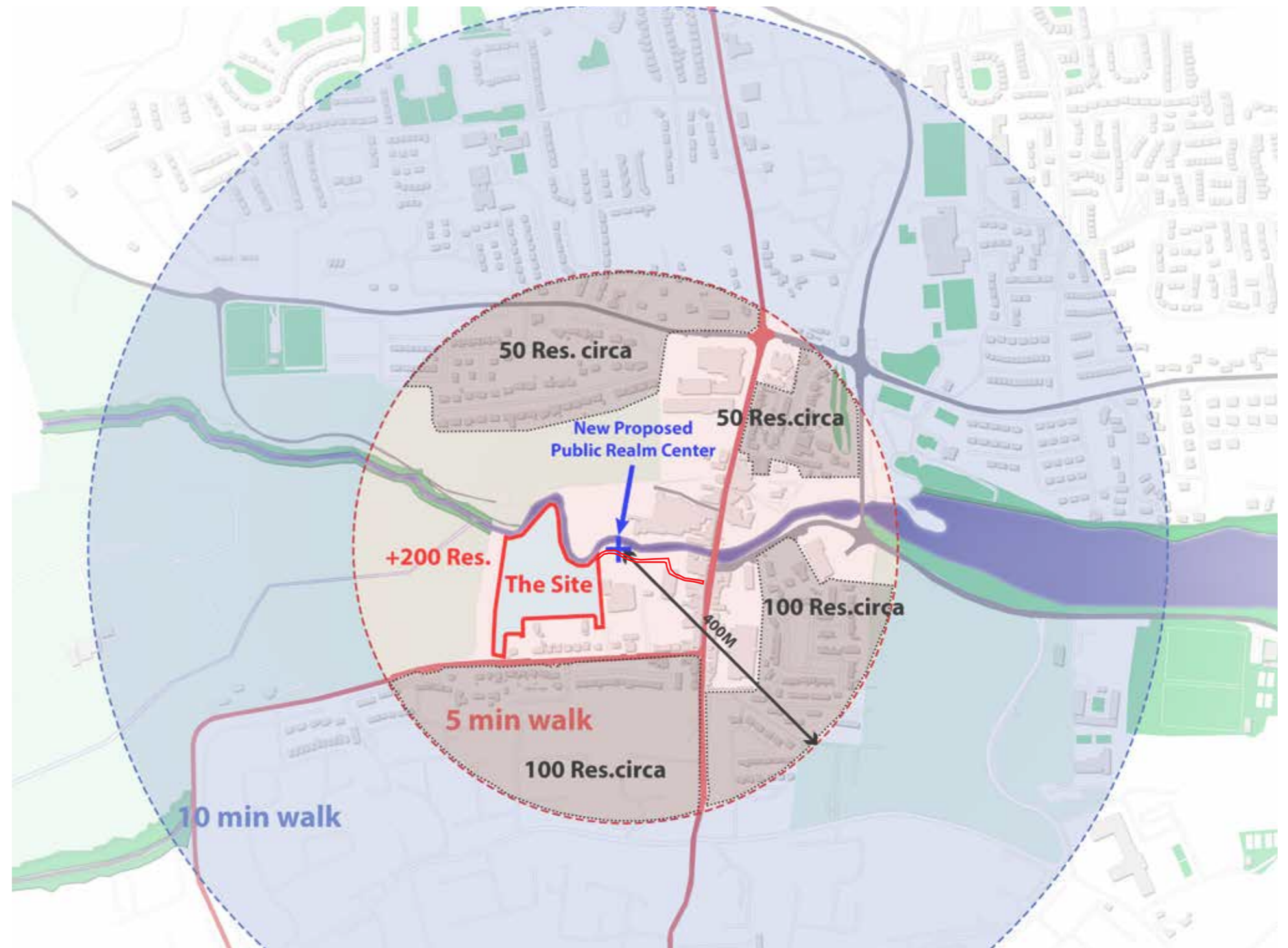


PEDESTRIAN SHED

From 'Design Manual for Urban Streets, Chapter 4 May.2016

The site is ideally located within the heart of Carrigaline town, and within the CCDP allocated town centre zone development. Creating a centralised development that has connectivity at it's core ...

- The pedestrian shed can be characterised by a radius of 0.25 miles which is the distance that is assumed a person is likely to walk before opting for their car.
- Although the majority of the catchment area of Carrigaline falls within a 5minute and 10 minute walking radius, journeys are significantly lengthened by streets not linking up. There is a plenty of opportunity for optimising the town for pedestrians by considering these links.
- At present the existing residential schemes within the 5min catchment area are composed predominantly of low density satellite suburbs with limited mixed uses. They receive no footfall aside from residents and therefore provide limited contribution to Carrigaline's town center or sense of place.
- Proposing a greater density residential scheme at the site along with mixed uses and appropriate public space would significantly contribute to development along the East West Access of the town and would improve on previous approaches.



Carrigaline Pedestrian Catchment illustrating existing residential schemes

4.4 EXISTING CAR PARKING



Existing Car Park utilised by the public in the town center

4.5 EXISTING OUTDOOR PUBLIC AMENITY SPACE



Existing outdoor public amenity space within the town

4.6 DESIGN PRINCIPLES



ANALYSIS AND APPROACH

In response to the site analysis and opportunities and constraints identified, the design strategy evolved into a few key design principles which would help inform the architectural rationale.

The key principles are as follows:

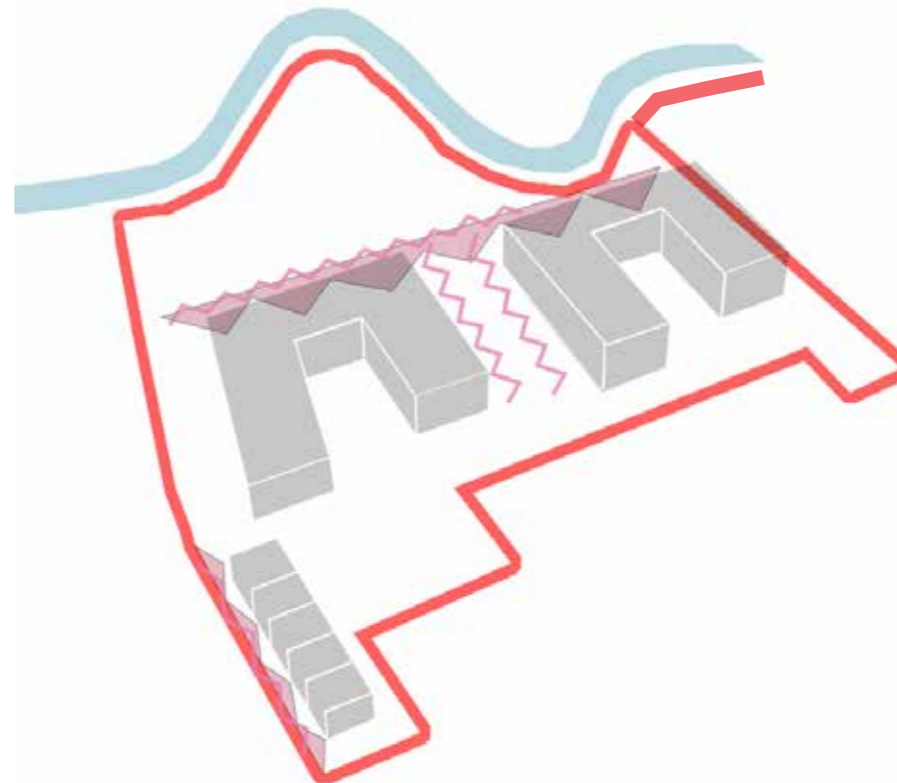
- **Principle 01: Maximise Amenity Space** - Aims to create a new public amenity space which will allow the development to provide permeability and accessibility through the site and into the wider context of the town.
- **Principle 02: Activate the Urban Edge** - Aims to activate the town centre to be in keeping with the growing population and urban development of Carrigaline. Providing a range of mixed use development on site that will benefit the town and future proof its growth.
- **Principle 03: Enhance Connectivity** - Aims to open up the site to the wider town to provide a more cohesive urban centre that comprises of multiple pedestrian access ways and routes. This aims to ensure the movement through the town functions more seamlessly as a whole rather than a start and finish point of movement.



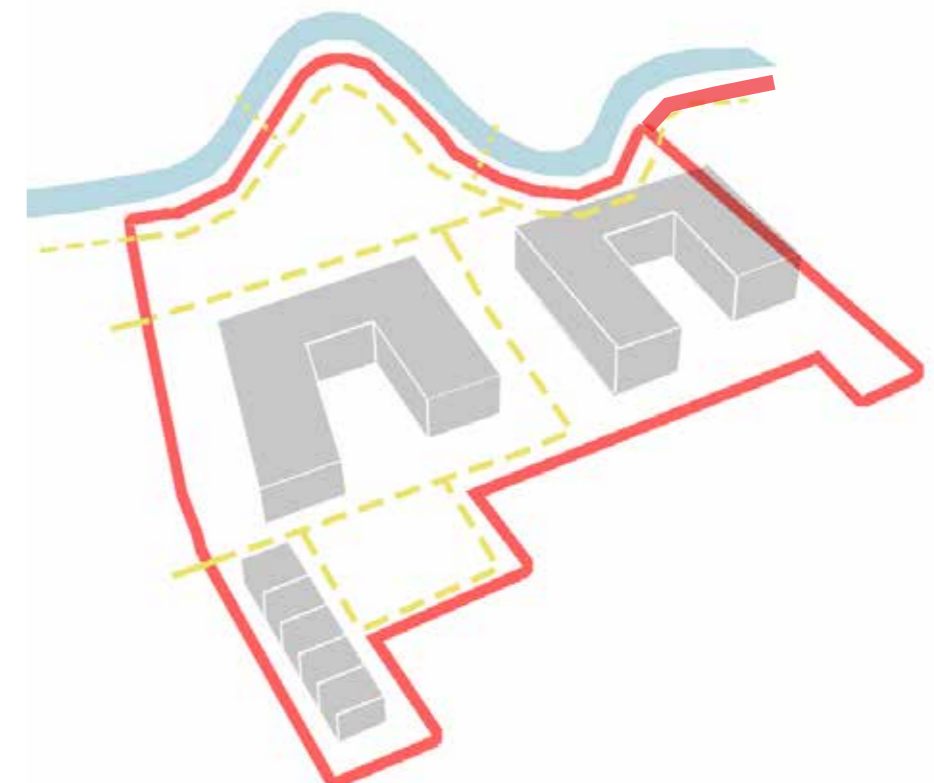
Principle 01: Maximise Amenity Space



Principle 02: Activate the Urban Edge



Principle 03: Enhance Connectivity



4.7 DESIGN DEVELOPMENT



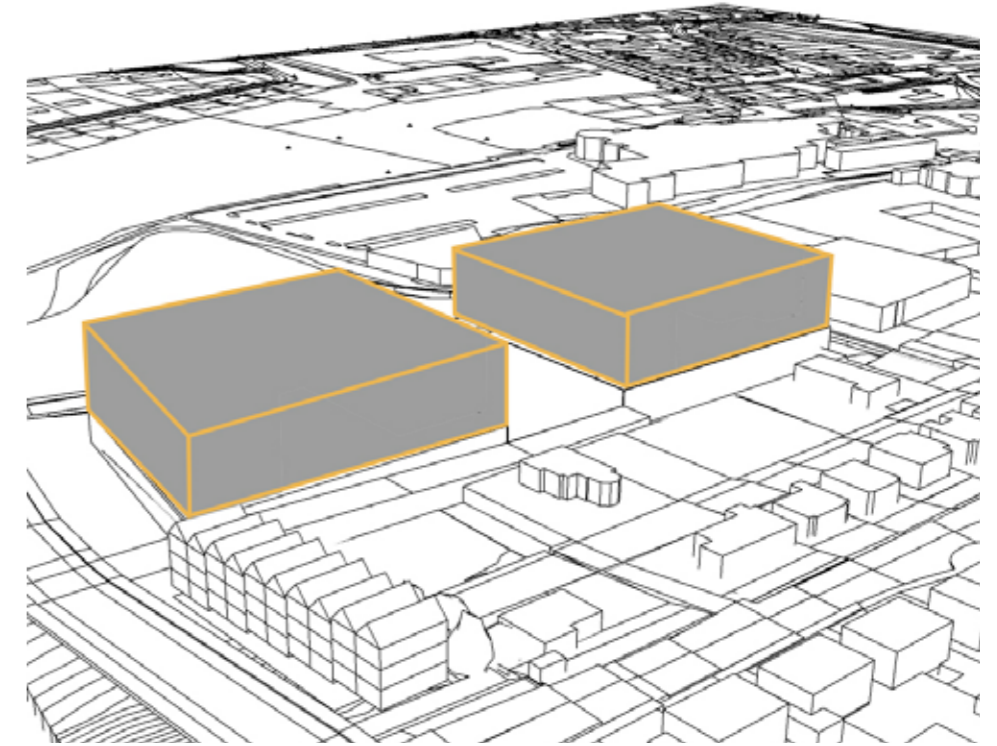
MASSING STUDY

The architectural composition has emerged through a process of considered design development and is informed by the established design principles and client brief.

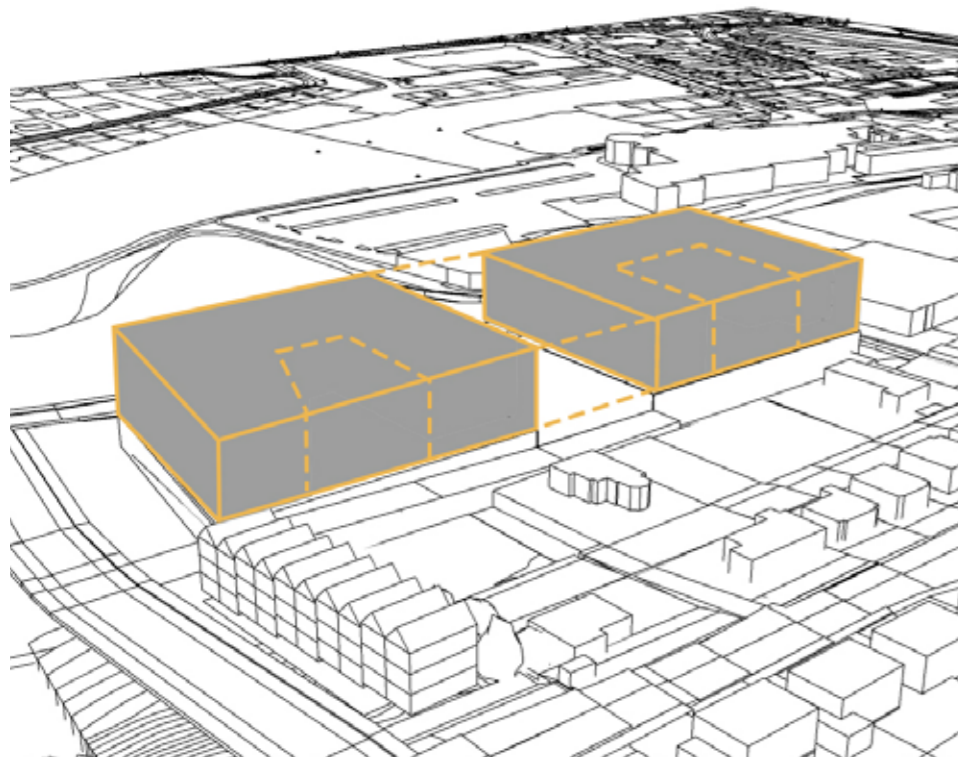
In order to fully utilise the site and unlock the rivers edge, the development has been positioned to the south of the site, thus keeping the north and river bank free for large outdoor amenity development. Starting with two large blocks, the building was gradually paired back and carved out to facilitate the client brief and promote amenity space, connectivity and consideration for the adjacent properties.

The development sits into the slope of the site, allowing two storeys of the building become submerged on the southern side of the site. The development builds up gradually from South to North, with the highest points on the North of the building, facing onto the large expanse of proposed amenity space and river frontage.

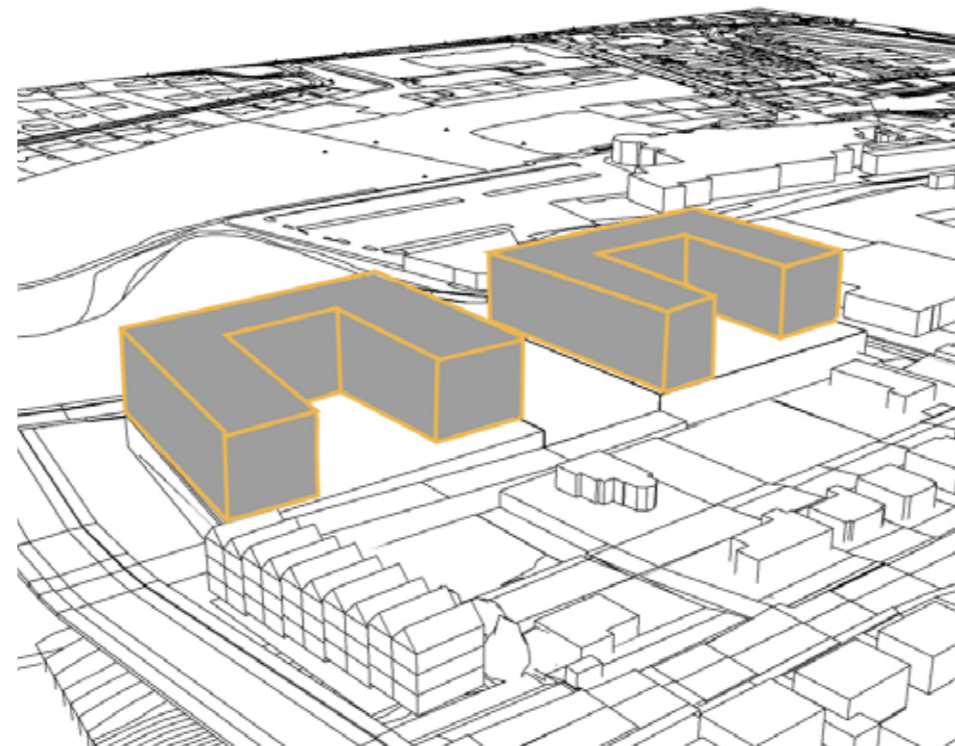
Massing Study - Full blocks



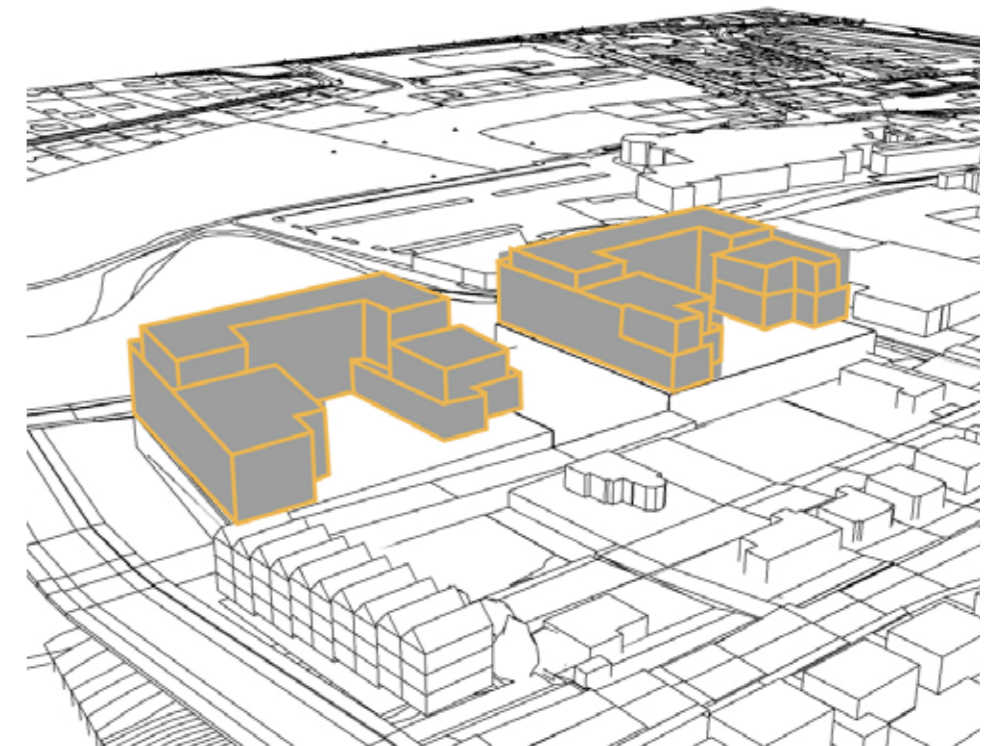
Massing Study - Divide up space



Massing Study - Carve out blocks



Massing Study - Push and pull blocks



4.7 DESIGN DEVELOPMENT



MASSING STRATEGY

A number of considerations have been made to fully satisfy the surrounding context as well as the client brief, which has resulted in the proposed massing form.

Important design approaches are as follows:

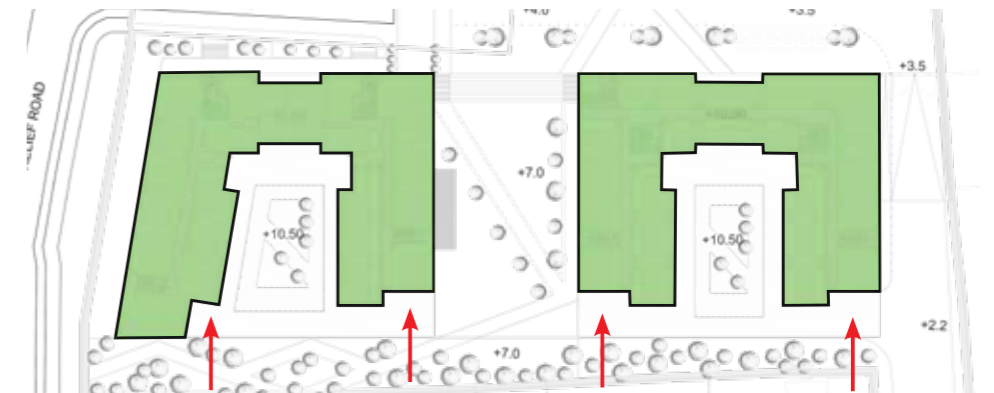
- Careful consideration has been given to the dwellings which reside in close to the site. The building naturally steps down with the existing topography to minimise elevation height to the south face of scheme facing dwellings.
- Penthouse and upper levels have been set back significantly to avoid potential overlooking.
- The massing of the building has also been set back following many investigative studies to enable maximise daylighting to the existing residents.
- Courtyards have been created to enable scheme to look inward to promote privacy for the existing dwellings.



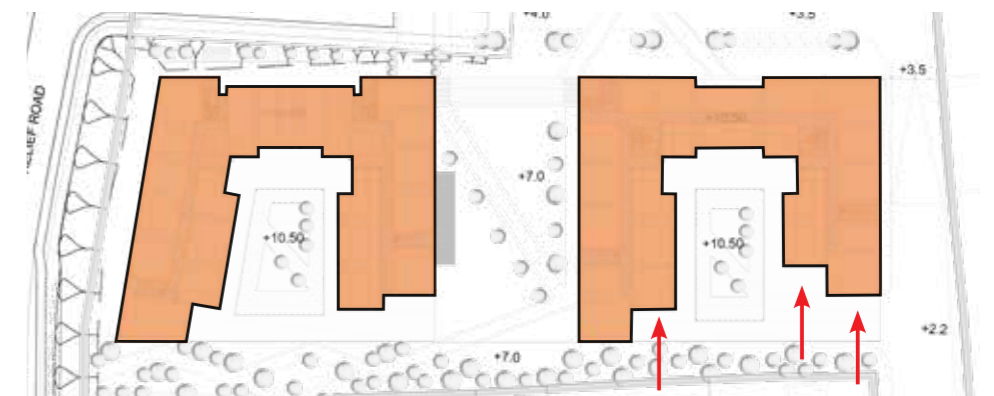
Adapting the design to accommodate the existing context



Design iteration - March 2021



Design iteration - April 2021

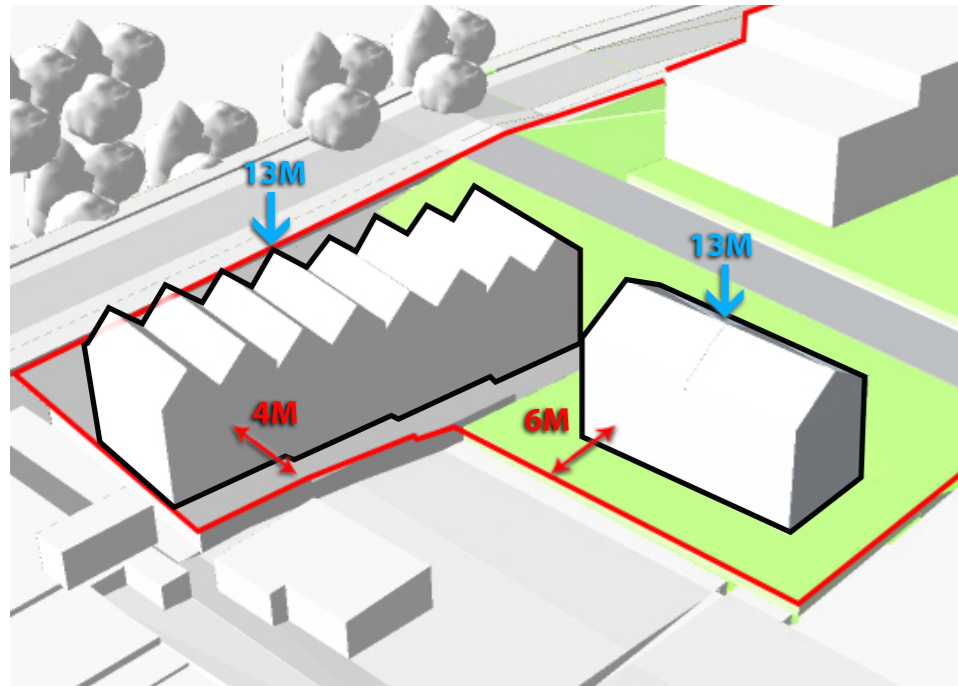


Design iteration - May 2021

4.7 DESIGN DEVELOPMENT

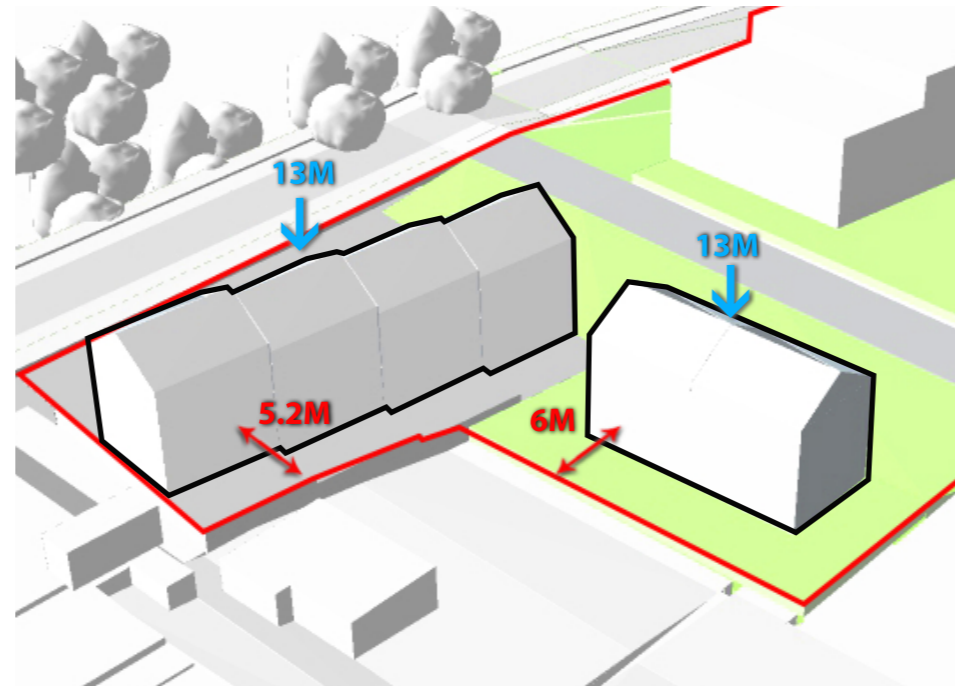


MASSING STRATEGY - RESPECTING BOUNDARIES



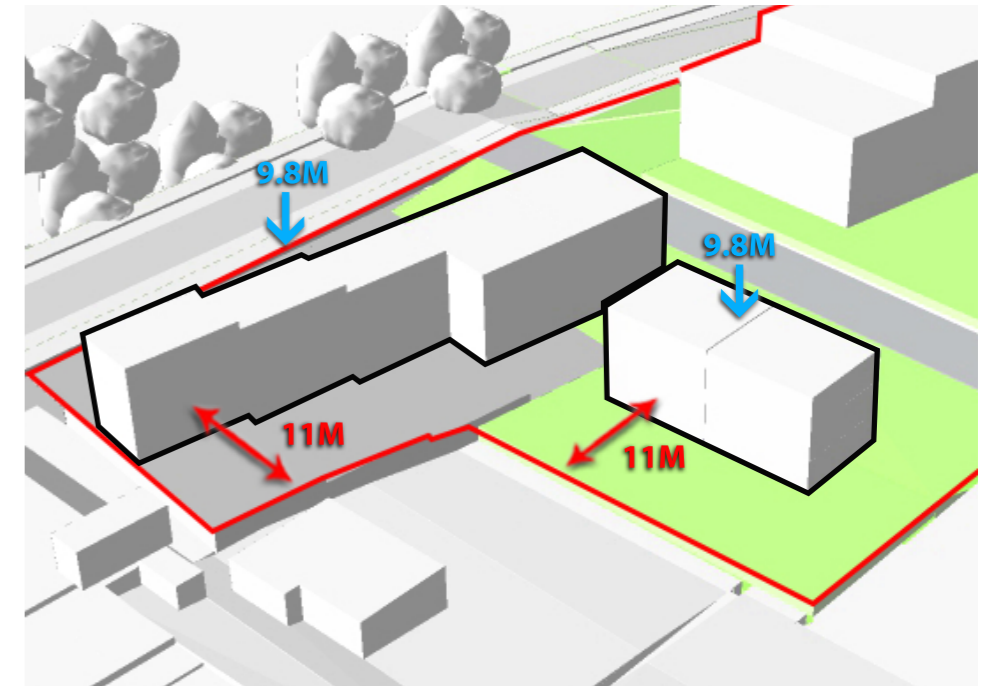
• Version 1 of Own Door Units/ Townhouses

- The initial proposal for the own door units to the south of the site was to create a proposal that would step down with the natural topography. We also wanted to create a form that would resemble that of a street terrace and reinforce the urban edge and add a richness of variety to the scheme.
- The challenges presented in these initial investigative massing exercises was that the form was hard to configure while maintaining appropriate dimensions to the boundary of the neighbouring sites



• Version 2 of Own Door Units/ Townhouses

- We understood from our initial massing studies that instead of the gable ends being emphasised on the front facing or east elevation we would instead have to rotate the form so that the gable ends would now face south.
- The shallower form proved easier to arrange program and to allow light to penetrate. However we still felt that the scheme was still imposing somewhat on the neighbouring boundary.



• Version 3 of Own Door Units/ Townhouses

- The final iteration of the scheme presents receding flat roofs which step down with the natural topography of the site. The elevation height is kept at 9.8M in order to significantly minimise the schemes impact on the neighbouring property.
- The distance of the scheme away from the boundary is maintained at a minimum of 11M to ensure adequate space and daylighting is provided.
- The units to the south of the scheme have contracted in depth to facilitate these minimum boundary distances.
- The L shape of the scheme presents a great opportunity for landscaping which will further contribute to the urban edge.

4.7 DESIGN DEVELOPMENT



OTHER STRATEGIES

- FACADE & BOUNDARY TREATMENT

In order to minimise impact on adjoining properties and respect to site boundaries, a number of design strategies have been explored and employed.

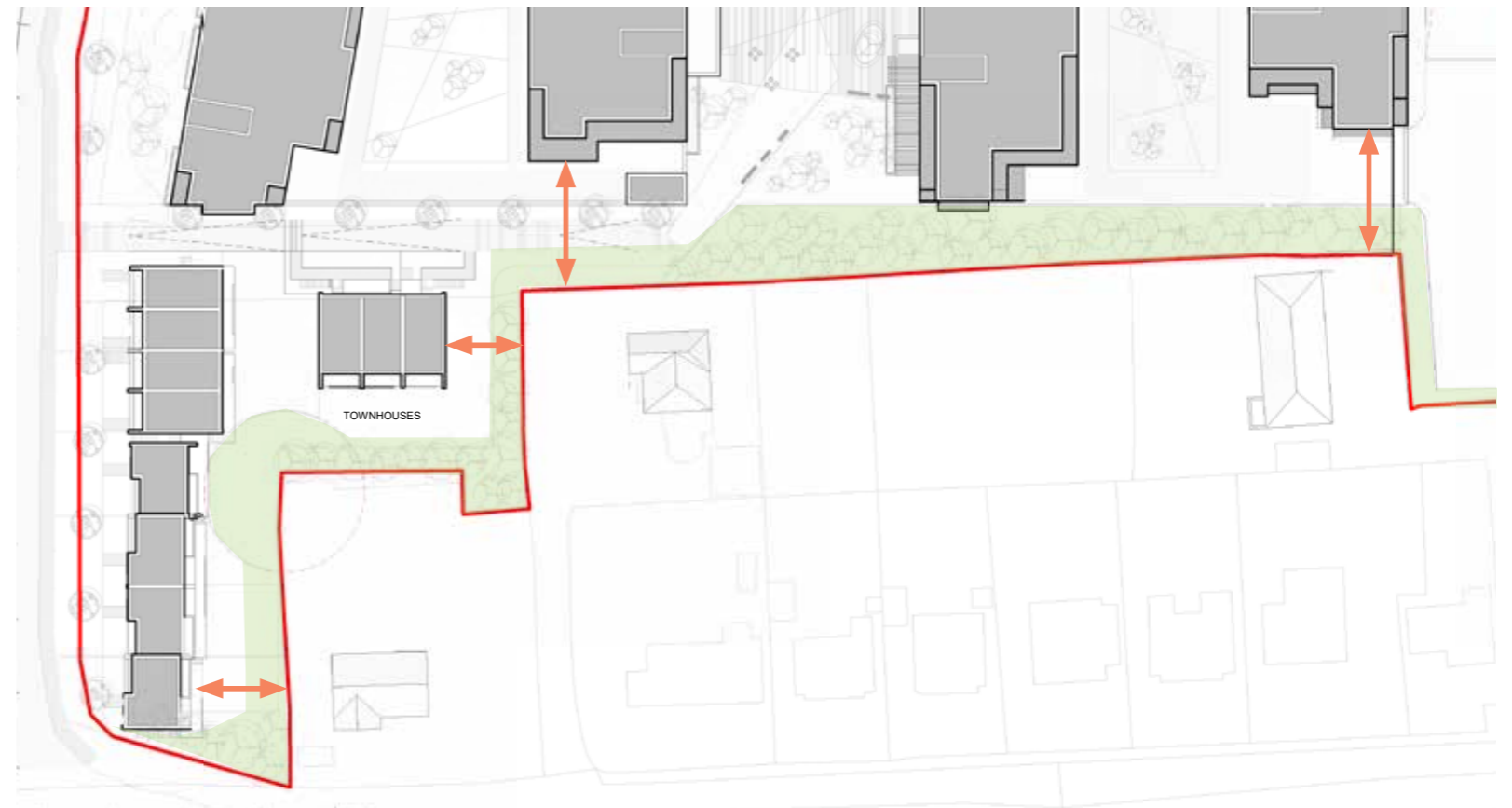
Important design strategies are as follows:

- Ensuring a min. of 11m distance from the development to the boundary with adjacent properties
- Providing adequate screening to all southern facades through means of hit and miss brick elements
- Avoiding glazing where at all necessary on the southern facade
- Providing adequate screening through carefully designed landscape along all boundaries to adjacent properties

Please see the *Residential Amenity Report* that accompanies this application for further information on design development strategies.



Hit and Miss brick details on southern facade



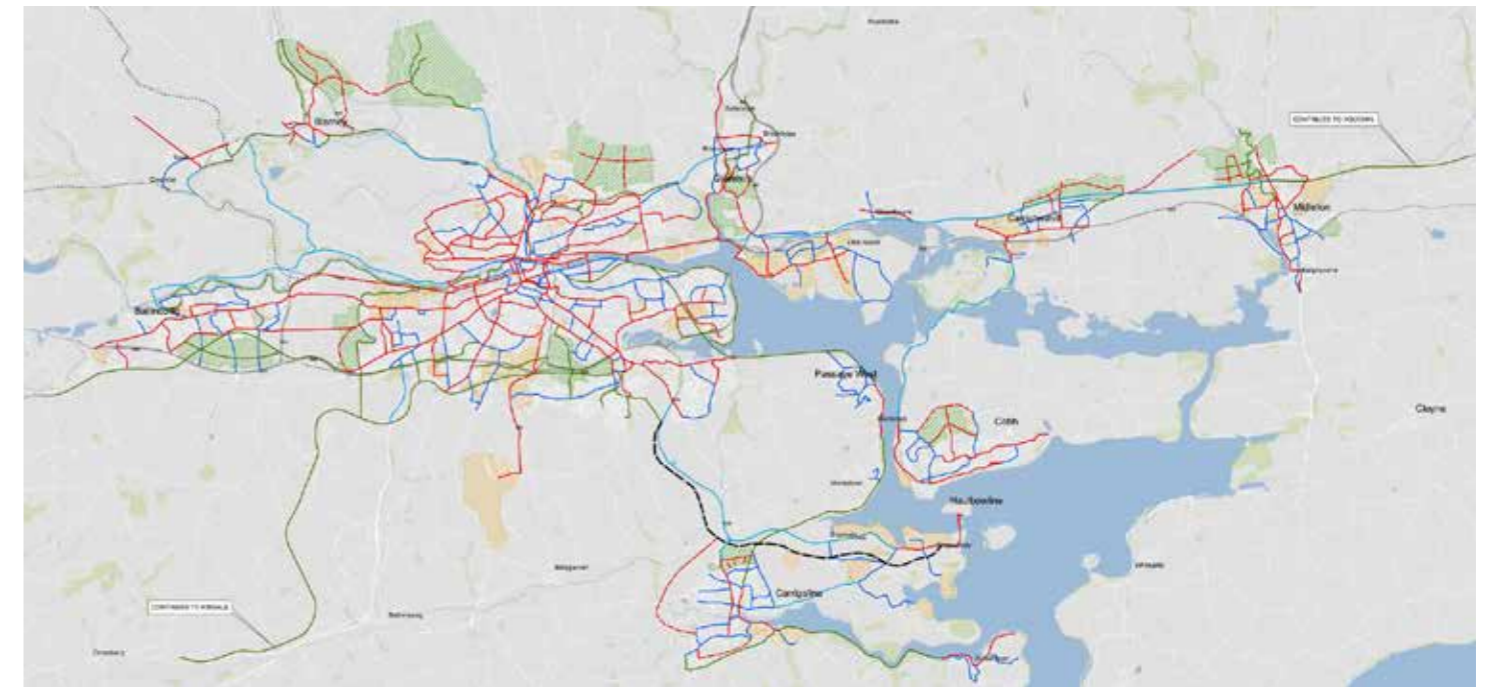
Landscape screening along southern boundary

4.7 DESIGN DEVELOPMENT



GREENWAY TO PROMOTE AMENITY

- There is significant lack of public amenity space to the west of Carrigaline town.
- The community centre and skate park are located to the East along with the old rail line amenity walk.
- The scheme looks to contribute to the public realm and provide for much needed amenity space to the west.
- Working alongside landscape architects, we are proposing a riverside park and greenway link for pedestrians and cyclists at this location



Cork County Cycle Network



Diagram indicating route of proposed greenway



Proposed greenway in context of Carrigaline town

4.7 DESIGN DEVELOPMENT



RESPONSE TO CORK COUNTY COUNCIL OPINION:

The following design developments have been made in response to the Cork County Council’s commentary during the pre-planning meeting of the project. A number of points were raised and addressed as follows:

1. ‘Overall site strategy explores emerging opportunities comprehensively with regard to connectivity with the town and adjoining un-developed lands. I would think that more vision is required around the concept of the connectivity with the town ensuring better quality of experience for pedestrian mobility and the relationship with the Owenabue River and adjoining private lands (Co-Op & Supervalu) where other quality development opportunities can be encouraged.’ – Greg Collins, Executive Architect, Cork County Council

- Provisions for a pedestrian link through the adjoining Dairygold site through to the main street has been proposed to encourage connectivity and permeability of the town and site. See full landscape drawings and report that accompany this application for further information and connectivity details.

2. ‘Careful design response required to mitigate against impact of adjoining single storey private dwellings located on the Kilmoney Road. Interface of new development to be considered for nearest 2 no. dwellings with regard to loss of day light, noise during construction, proposed boundary treatments and proposed ground levels located immediately North of the site boundary.’ – Greg Collins, Executive Architect, Cork County Council

- Please see adjoining document ‘Residential Amenity Report’ for a full breakdown of how this has been addressed.

3. ‘Greenway and public realm, use and defensibility strategy to be developed further in order to distinguish between private / semi-private and public areas.’ – Greg Collins, Executive Architect, Cork County Council

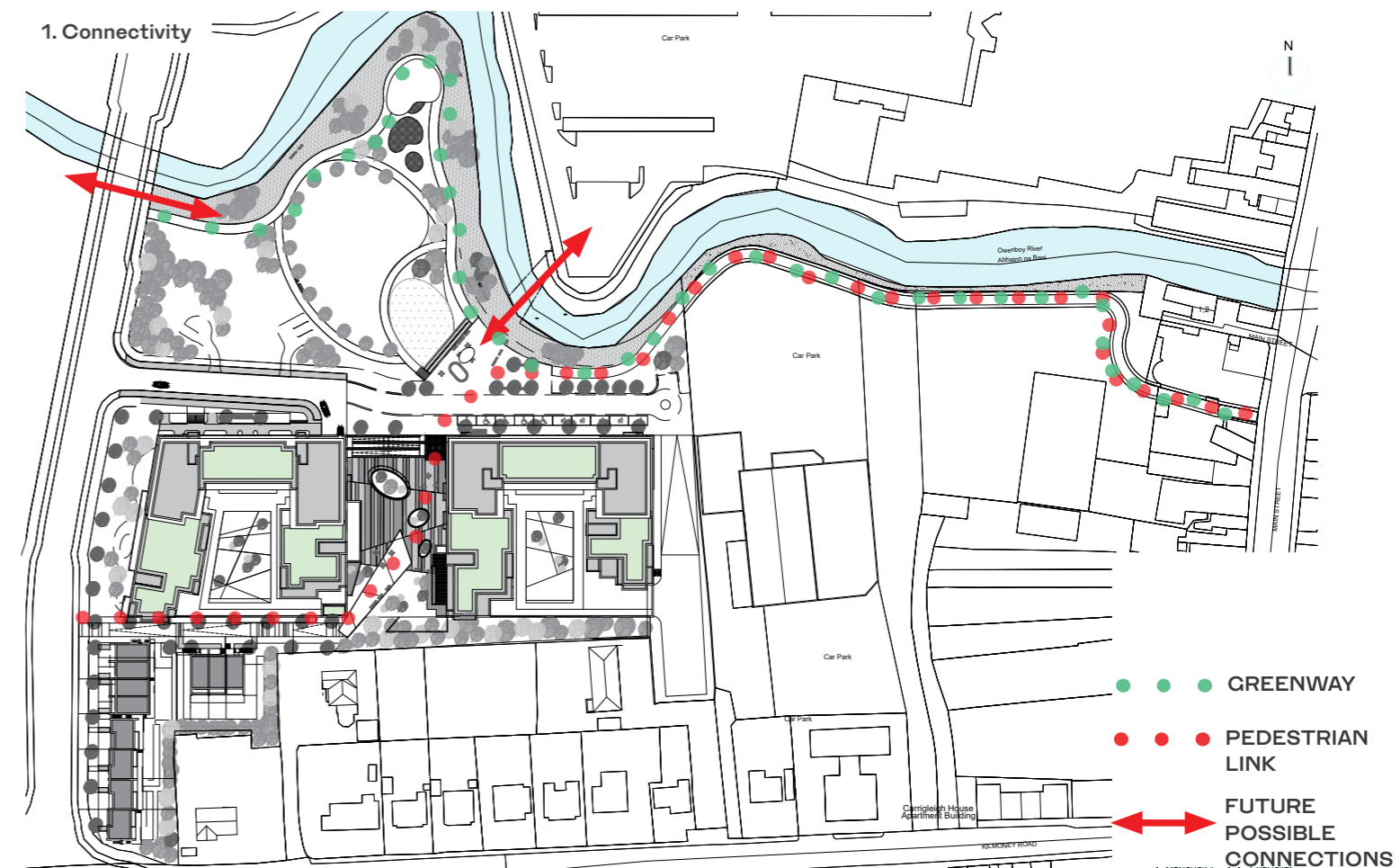
- Greenway and public realm areas have been defined through the use of level changes, screening and clever landscaping solutions. See full landscape drawings and report that accompany this application for further information.

4. ‘Design impact of dwelling(s) located at new junction between new Western Relief Rd & the Kilmoney Road where rear private garden space with raised garden areas? / steps are visible from the Kilmoney Road?’ – Greg Collins, Executive Architect, Cork County Council

- Landscape design of the Townhouses has been carefully planned out and can be seen in detail in the adjoining landscape documents and drawings as above.

5. ‘Various Floor plans in the apartment blocks indicate long unfriendly internal access corridors that will need artificial lighting and ventilation. Floor plan re-design needed to remove these unhealthy elements of the plan.’ – Greg Collins, Executive Architect, Cork County Council

- The need to minimise long corridors has been addressed where possible within the development with glazing provided for at the end of these to maximise light. See full set of architectural drawings accompanying this application for full development layouts.



4.7 DESIGN DEVELOPMENT



RESPONSE TO CORK COUNTY COUNCIL OPINION:

6. *‘Some of the internal courtyard images have stark looking elevational treatments, large high wall sections with poor sense of human relationship. Better attention to a more people centred development needed here. More variety of external brick colours could be used as the CGI images are dominated by a light brown / buff colour. Lacks vibrancy, vitality.’*

– Greg Collins, Executive Architect, Cork County Council

- The elevations have been developed to be easily maintained and of high quality and locally sourced material finishes where possible. The large facades without glazing have been detailed with brick texture to create some variation and mitigate starkness. Please see section 5.6 Materiality of this report for further information on this.

7. *‘External elevational treatments are impressive but concern about maintenance/ cleaning of large full length glazed balcony screens requiring expensive end-user management.’* – Greg Collins, Executive Architect, Cork County Council

- The elevations have been developed to be easily maintained and of high quality and locally sourced material finishes where possible. Please see section 5.6 Materiality of this report for further information on this.

8. *‘Ventilation of under-ground car parks and integration/ location of plant for same as all elevations as indicated are active with residential components.’* – Greg Collins, Executive Architect, Cork County Council

Architect, Cork County Council

- Ventilation has been provided for at ground and first floor level through the use of natural ventilation systems. See drawing P1010 and P1011 of the architectural drawing pack for full floor layouts and accompanying M&E report from Horizon Engineering.

9. *‘Ensure all plant areas/ goods delivery areas associated with retail units are well separated from residential access/ public realm areas and do not infringe on privacy and amenity of residential areas.’* – Greg Collins, Executive Architect, Cork County Council

- Plant areas have been concealed and contained away from residential and public access areas that serve both the retail and residential sections of the development. See drawing P1010 and P1011 of the architectural drawing pack for full floor layouts and accompanying M&E report from Horizon Engineering.

10. *‘Need for community facilities on site to be central part of the development and be detailed to a high standard.’* – Greg Collins, Executive Architect, Cork County Council

- The provision of community facilities within the development have been addressed at the first floor podium level. See drawing P1011 of the architectural drawing pack for full floor layout.

11. *‘Need for design of units to be suitable for multi-generational living with allowance for step down units for aged community. Will an AHB be involved here? Ideally ‘step down’ or special needs units should be at ground level and have small private integrated gardens as part of their floor plan (like an integrated balcony/ winter garden) that will look onto public realm areas. Appropriate compliant/ high standard of design should apply to all units and external public realm areas. Dedicated aged friendly parking be located as close as possible to the development (not in the underground car park) to provide for residents with mobility issues. Examine relationship of respective parking to serve the proposed duplex units.’* – Greg Collins, Executive Architect, Cork County Council

Architect, Cork County Council

- A variety of unit types have been provided across the scheme - 1 Bed/2 Bed/3 Bed units - with a number of them located at ground floor level and suitable for a number of different demographics. See the ‘Housing Quality Assessment Report’ that accompanies this application for further information on unit types.

12. *‘Details of bins stores / and all building utilities to be seamlessly integrated into the design using high quality sustainable materials.’* – Greg Collins, Executive Architect, Cork County Council

Council

- Centralised managed secure internal bin store at ground floor level. Apartment heat pumps centrally located at roof level for optimum efficiency and to facilitate ease of maintenance and replacement. Parapet height has been co-ordinated with plant design to ensure plant is not visible on the roof. All materials will be of high quality and sustainably sourced using local materials where possible.

5.0 DESIGN PROPOSAL

5.1 PROPOSAL OVERVIEW



PROPOSED DEVELOPMENT

The proposed development comprises of 26,557m² (GIA) two 7 storey buildings and 11 townhouse style buildings to accommodate 224 residential units across the site. This includes understorey car parking at ground and first floor, along with retail units at ground floor, residential support units and community focused units at first floor level.

Outdoor landscaped residential courtyards are located at second floor level, with a central podium courtyard located at first floor level that is for public and community use. A large portion of the site to the North is an extensive public amenity space that will connect in the greenway proposed for Carrigaline, and house a number of walking paths, ball areas, outdoor gym equipment area and a small retail kiosk for food and beverage use.

The ground level of the site will be brought up to +4.000 OD, which brings floor levels above the flood level. A gently sloped access route will link through from the townhouses and Western Relief Road and down on to the central podium courtyard, maximizing connectivity through the site. This will act as a 'pedestrian corridor' from one end of the site through to the open public amenity space and greenway that connects with the wider town centre.



3D model of proposed development

5.2 SITE LAYOUT



ARCHITECTURAL APPROACH

The proposed development centres around connectivity through the site. The two main apartment blocks are U-shaped and orientated around external courtyards, with a large public amenity space to the north. A central podium deck runs through the centre, dividing block one and two and creating a thoroughfare linking the townhouses through the main development down to the public park space adjacent to the rivers edge.

Connectivity of space within the development is paramount as well as connectivity of the site with the wider context of Carrigaline town.

The development is quite urban in terms of architectural design - activating street fronts and creating an urban edge is important for the site's location and this is represented through the design - a number of retail facilities are located on the ground floor, with childcare facilities accessible via the central podium deck. Residential facilities such as a gym and residential work lounge spaces are located off the podium deck also. All apartments are located from the 2nd storey up to the 6th storey where penthouse style apartments are located.

As you move south along the site, the townhouses become more domestic in scale, with individual own door access to all these units which maintains the active street front with the western relief road.



Proposed Site Layout Plan

5.3 GREENWAY AND PUBLIC REALM



LANDSCAPE STRATEGY

The landscape design strategy for the development focuses on the following:

- integrating the development in the wider context/town centre of Carrigaline
- providing an extensive open and inviting outdoor public amenity space for the town
- enhancing pedestrian permeability and ease of movement through the site
- promotes and facilitates walking and cycling in line with principles of DMURS
- providing a sustainable design approach when dealing with the flood planes on site
- promoting the preservation of the natural rivers edge
- connecting the site with adjacent sites through means of the introduction of a greenway line



Landscape Masterplan

5.3 GREENWAY AND PUBLIC REALM



AMENITY TYPOLOGIES



Semi-Private Amenity Space - Shared Courtyard



Public Amenity Space - Active Street Space



Public Amenity Space - Parkland lawn and meadow



5.4 TOPOGRAPHY



Overcoming site constraints

A key feature of the site is its sloping topography. The site falls north to south beginning at a Spot Level of 11.0D at the north end and terminating at 2.0D at the southern end.

The design approach carefully responds to this by stepping the proposal down to gain additional height. Ground floor units can all be accessed at grade level without the need for additional steps.

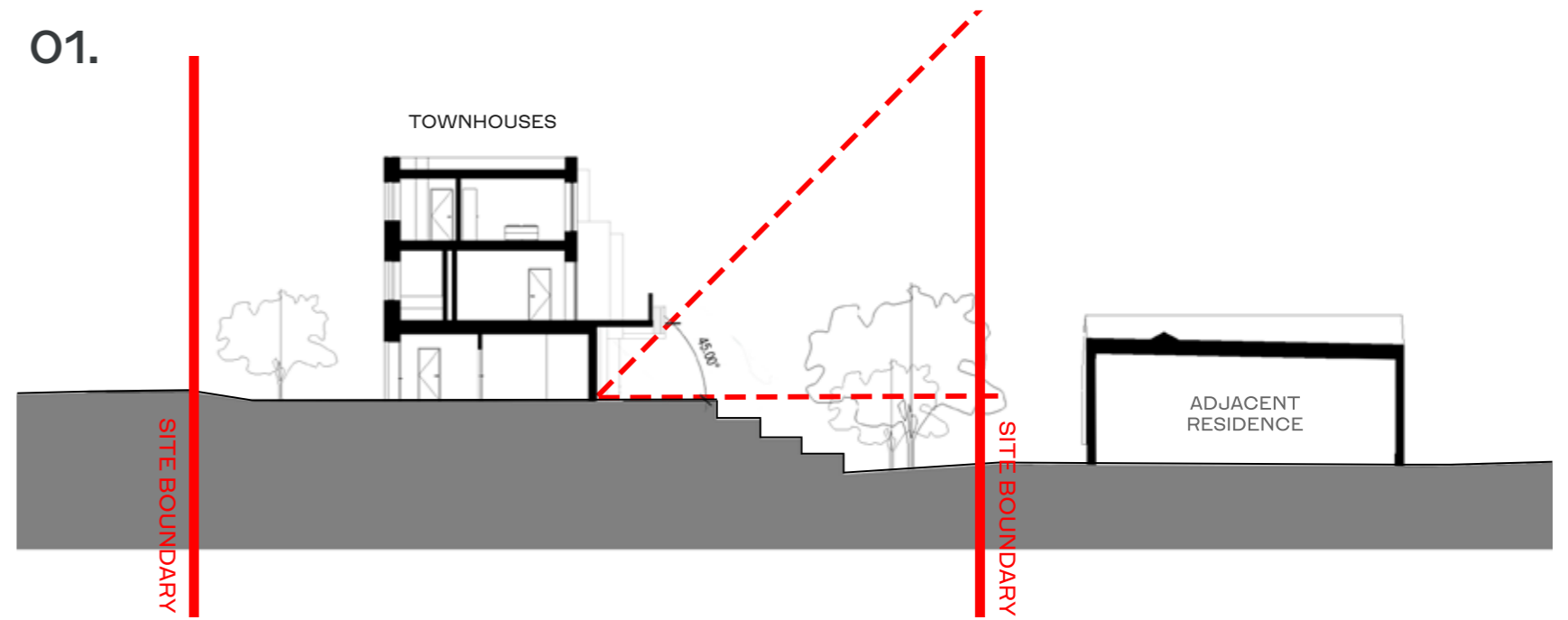
The building forms step back appropriately to respond to nearby residential units. The upper levels also recede significantly to avoid overlooking and this creates green terraces to promote biodiversity and enhance the environmental approach. The diagrams here demonstrate the 45 degree angle taken from each residence and show how the step back of the building mitigates any over-shadowing and over-looking.

Please see the *Residential Amenity Report* and *Building Height Rationale* that accompanies this application for further information on design strategies.

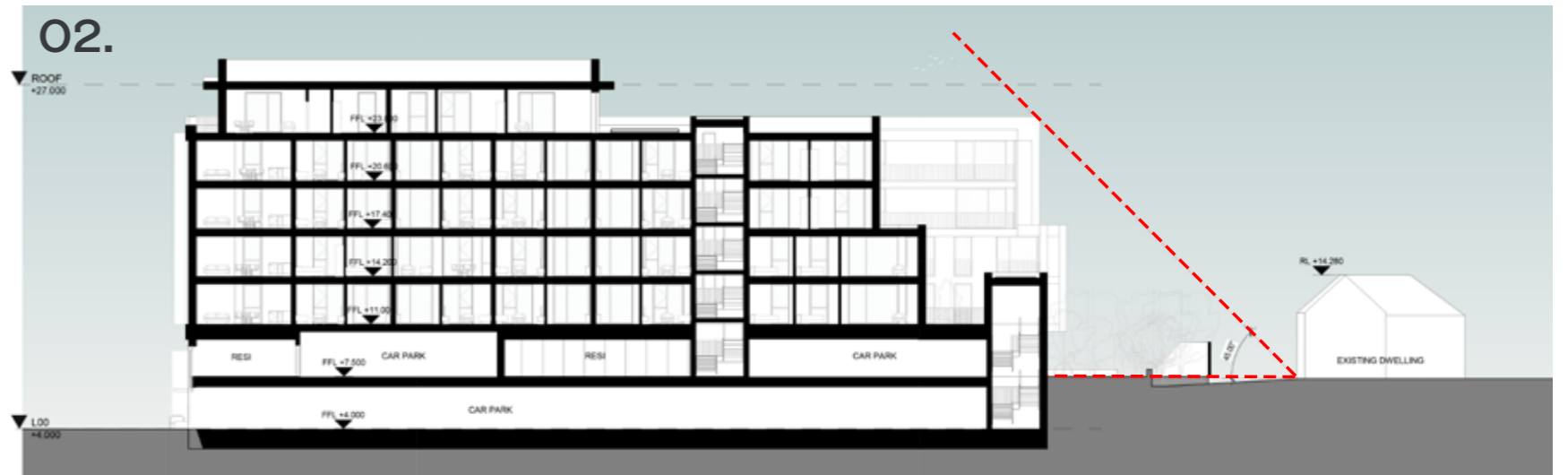


KeP Plan showing section lines

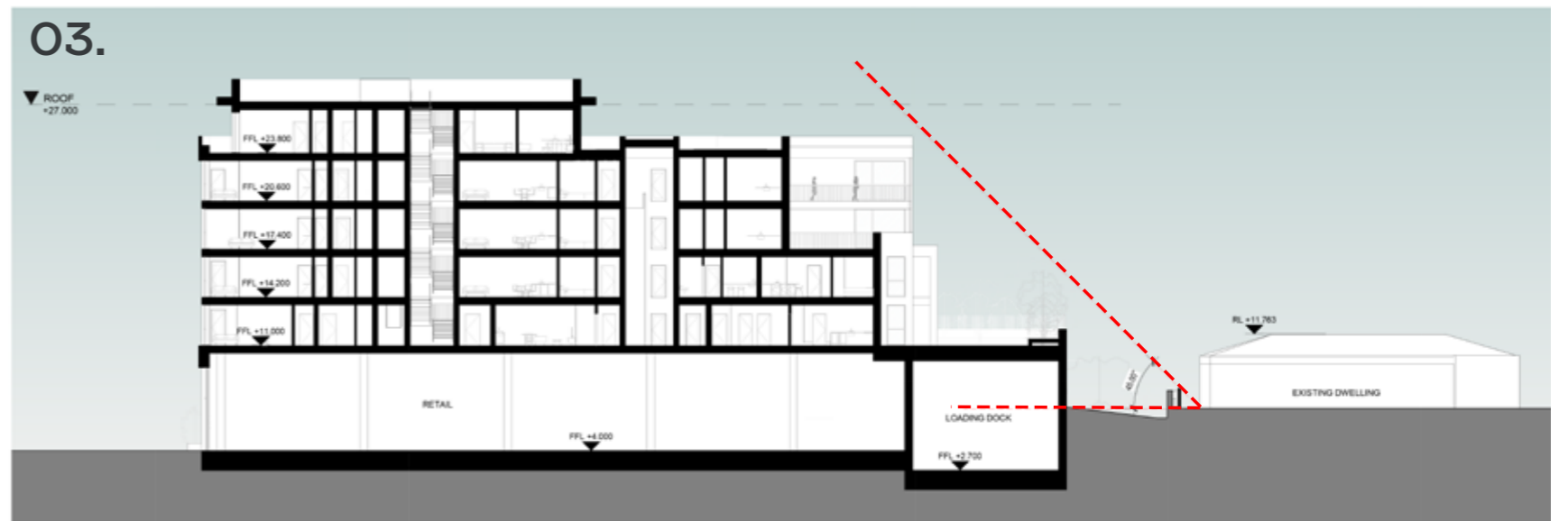
01.



02.



03.



Sections demonstrating topography and adjacent residential sites

5.5 PROPOSED PLANS



GROUND FLOOR PLAN



5.5 PROPOSED PLANS



TYPICAL APARTMENT FLOOR LAYOUT



Plan Layout

- 1 BED
- 2 BED (3 PERSON)
- 2 BED (4 PERSON)
- 3 BED

5.5 PROPOSED PLANS



PENTHOUSE APARTMENT FLOOR LAYOUT



Key Plan

Plan Layout

- 1 BED
- 2 BED (3 PERSON)
- 2 BED (4 PERSON)
- 3 BED

5.6 MATERIALITY



PALETTE



Bluff brick



Brick texture



Hit and Miss brick - privacy



Metal work



Smokey green metal to accent windows and balustrades



Metal balustrades to balconies

5.6 MATERIALITY



FACADE TREATMENT

ALL DIMENSIONS TO BE CHECKED ON SITE
NO DIMENSIONS TO BE SCALED FROM THIS DRAWING
DRAWING IS TO BE READ IN CONJUNCTION WITH RELEVANT



BUILDING BLOCK 01

ROAD

TOWN HOUSES

1 GA - ELEVATION - WEST
1:200



BUILDING BLOCK 02

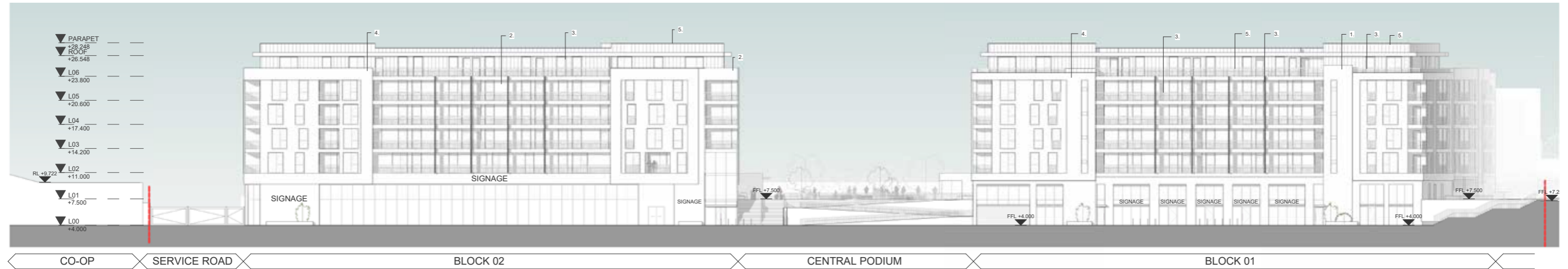
ROAD

2 GA - ELEVATION - WEST (PODIUM)
1:200

MATERIAL LEGEND

1. Reconstituted / natural stone cladding - limestone or similar in dark grey colour
2. Brick cladding - stretched bond in buff colour
3. Powder coated metal balustrades / screens - green/grey colour
4. Reconstituted / natural stone cladding - light grey colour
5. Metal cladding system with standing seam - natural zinc or similar
6. Hit and miss brick facade
7. Powder coated aluminium louvres

5.7 PROPOSED CONTEXTUAL ELEVATIONS



1 GA - ELEVATION - NORTH
1 : 250

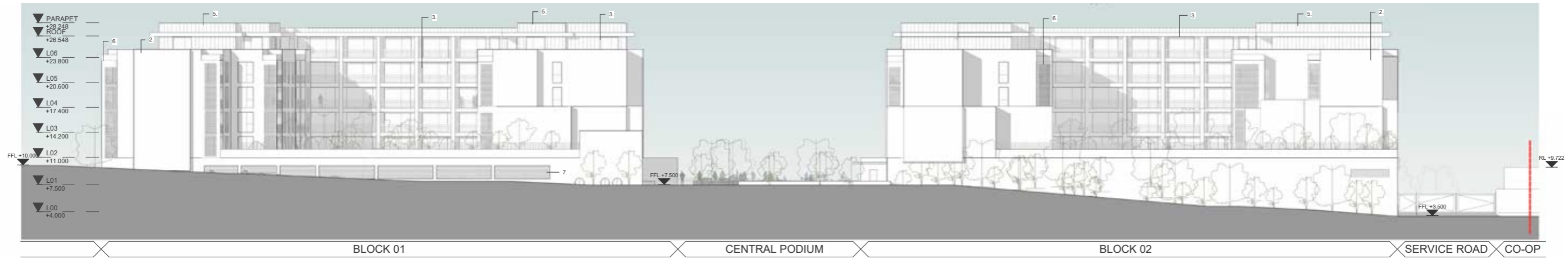


1 GA - ELEVATION - EAST (PODIUM)
1 : 200

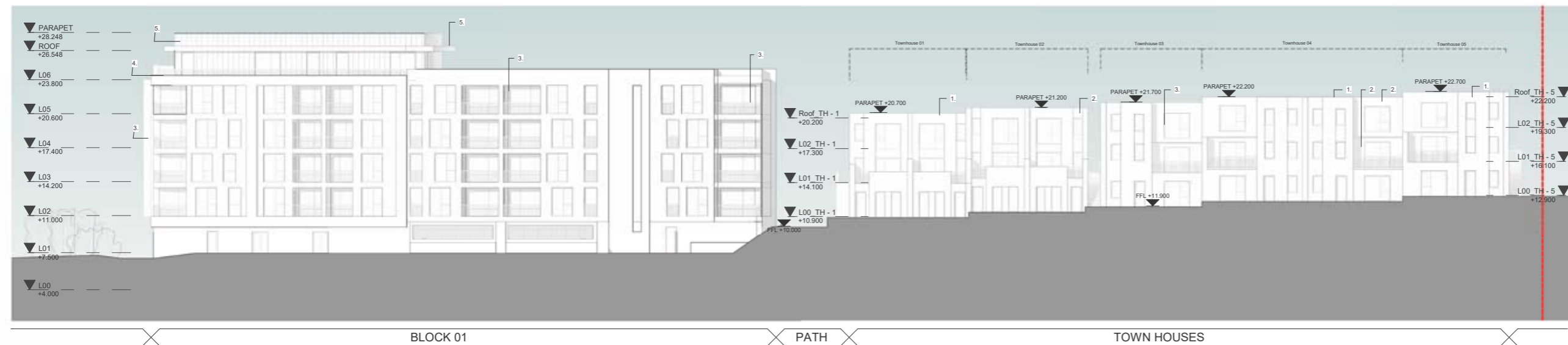
MATERIAL LEGEND

1. Reconstituted / natural stone cladding - limestone or similar in dark grey colour
2. Brick cladding - stretched bond in buff colour
3. Powder coated metal balustrades / screens - green/grey colour
4. Reconstituted / natural stone cladding - light grey colour
5. Metal cladding system with standing seam - natural zinc or similar
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7. Powder coated aluminium louvres

5.7 PROPOSED CONTEXTUAL ELEVATIONS



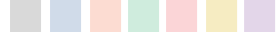
2 GA - ELEVATION - SOUTH
1 : 250



- MATERIAL LEGEND**
1. Reconstituted / natural stone cladding - limestone or similar in dark grey colour
 2. Brick cladding - stretched bond in buff colour
 3. Powder coated metal balustrades / screens - green/grey colour
 4. Reconstituted / natural stone cladding - light grey colour
 5. Metal cladding system with standing seam - natural zinc or similar
 6. Hit and miss brick facade
 7. Powder coated aluminium louvres

1 GA - ELEVATION - WEST
1 : 200

5.8 FINAL RENDER IMAGES



VIEW FROM OUTDOOR AMENITY SPACE TOWARDS APARTMENT
BLOCKS 01 & 02

5.8 FINAL RENDER IMAGES



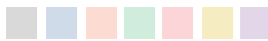
VIEW OF CENTRAL PODIUM COURTYARD

5.8 FINAL RENDER IMAGES



VIEW OF NORTH ELEVATION OF APARTMENT BLOCK 01 & 02

5.8 FINAL RENDER IMAGES



VIEW OF PEDESTRIAN WALKWAY THROUGH DEVELOPMENT

5.8 FINAL RENDER IMAGES



VIEW OF TOWNHOUSES ALONG THE WESTERN RELIEF ROAD

5.8 FINAL RENDER IMAGES



VIEW OF DEVELOPMENT FROM WESTERN RELIEF ROAD

6.0 SCHEDULE

6.1 SCHEDULE OF ACCOMMODATION



Carrigaline SHD - Schedule of Accommodation				May-22				
Site Area	Main Site							
	(sqm)							
(Red Boundary Line)	30701							
Proposal	Footprint	GIFA	No. Of Units					
	(sqm)	(sqm)						
Apartment Blocks & Retail (Not incl. Car Park)	9250	23886	202 Units					
Townhouses	867	2034	22 Units					
Total	10117	25920	224Units					
Apartment Blocks & Retail								
Level O0	GIFA	Info		1 Bed	2 Bed (3 Person)	2 Bed (4 Person)	3 Bed	Total
	(sqm)							
Car Parking (Undercroft)	4784	157 Spaces						
Retail	3158	Main Retail Unit: 3000sqm Unit 1: 67 sqm Unit 2: 91sqm						
Residential	664	(Cores/Ancillary etc.)						
Total	8606							
Level O1	GIFA	Info		1 Bed	2 Bed (3 Person)	2 Bed (4 Person)	3 Bed	Total
	(sqm)							
Car Parking	2809	88 Spaces						
Childcare	184	184sqm Internal 164sqm External						
Residential (Amenity)	842	(Cores/Gym/Ancillary/Amenity)						
Total	3835							
Level O2	GIFA	Info		1 Bed	2 Bed (3 Person)	2 Bed (4 Person)	3 Bed	Total
	(sqm)							
Residential	4387	Block 1: 2214sqm / 23 units Block 2: 2173sqm / 22 units		16	4	15	1	36
			Part V	3	1	5		9
Total	4387			19	5	20	1	45

6.1 SCHEDULE OF ACCOMMODATION



Level	GIFA (sqm)	Info	1 Bed	2 Bed (3 Person)	2 Bed (4 Person)	3 Bed	Total	
Level 03								
Residential	4423	Block 1: 2214sqm / 24 units Block 2: 2140sqm / 23 units	21	2	16	2	41	
		Part V	1	2	3		6	
Total	4423		22	4	19	2	47	
Level 04								
Residential	4108	Block 1: 2138sqm / 24 units Block 2: 1970sqm / 20 units	20	2	13	3	38	
		Part V	1	2	2	1	6	
Total	4108		21	4	15	4	44	
Level 05								
Residential	4108	Block 1: 2138sqm / 24 units Block 2: 1970sqm / 20 units	20	3	15	4	42	
		Part V	1	1			2	
Total	4108		21	4	15	4	44	
Level 06								
Residential	2012	Block 1: 997sqm / 11 units Block 2: 1015sqm / 11 units	11	1	9	1	22	
		Part V					0	
Total	2012		11	1	9	1	22	
			1 Bed	2 Bed (3 Person)	2 Bed (4 Person)	3 Bed	Total	
Total Part V Units			6	6	10	1	23	
Overall Area Total	31479		Overall Total Units	94	18	78	12	202
			<i>Percentage Mix (%)</i>	<i>46.5</i>	<i>8.9</i>	<i>38.6</i>	<i>5.9</i>	

6.1 SCHEDULE OF ACCOMMODATION



Townhouses							
Level	GIFA	Info	1 Bed	2 Bed (3 Person)	2 Bed (4 Person)	3 Bed	Total
Level -01	(sqm)						
Residential	242	Duplex Units Lower					
Total	242	Units: (2A/2B/3A/4A)					
Level 00	(sqm)						
Residential	663	1 Bed Units & Duplexes	7		4		11
Total	663	Units: (1A/1B/2A/2B/3A/4A/4B/5A/6A/6B/6C)* *1 Beds highlighted in Bold					
Level 01	(sqm)						
Residential	567	Duplex Units Lower			11		11
Total	567	Units: (1C/1D/2C/2D/3B/4C/4D/5B/6D/6E/6F)					
Level 02	(sqm)						
Residential	562	Duplex Units Upper					
Total	562	Units: (1C/1D/2C/2D/3B/4C/4D/5B/6D/6E/6F)					
Overall Total	2034						Units: 22

Overall Scheme Total	GIFA	Info	Percentage
	(sqm)		(%)
Retail	3158		12.2
Childcare	184		0.7
Residential	22578	224 units	87.1
Total	25920		100